

REGIONAL DISTRICT OF NANAIMO
REGIONAL PARKS AND TRAILS SELECT COMMITTEE
AGENDA

Tuesday, February 5, 2019

12:00 P.M.

Board Chambers

This meeting will be recorded

Pages

1. CALL TO ORDER
2. APPROVAL OF THE AGENDA
3. ADOPTION OF MINUTES
 - 3.1 Regional Parks and Trails Select Committee Meeting - October 9, 2019 3

That the minutes of the Regional Parks and Trails Select Committee meeting held October 9, 2018 be adopted.
4. PRESENTATIONS
 - 4.1 Regional Parks and Trails Orientation Presentation
5. DELEGATIONS
6. CORRESPONDENCE
 - 6.1 Save French Creek Estuary Land, re: French Creek Estuary Land Introduction 6
 - 6.2 R. Robinson, Friends of French Creek Conservation Society, re: Report on the French Creek Estuary Lands and Memorandum of Understanding 7
7. UNFINISHED BUSINESS
8. REPORTS
 - 8.1 Parks Update Report - July-September 2018 18

That the Parks Update Report - July - September 2018 be received as information.

- 8.2 Parks Update Report – October-December 2018 33**
- That the Parks Update Report – October-December 2018 be received as information.
- 8.3 Little Qualicum River Regional Park Bridge Replacement Detailed Design and Class B Costing 51**
1. That the Little Qualicum River Regional Park Bridge Replacement proceed to Tender in February 2020 for construction in 2020.
2. That the 2020-2024 Financial Plan include \$839,910 in the 2020 year for completion of the LQRRP Bridge Replacement.
- 9. BUSINESS ARISING FROM DELEGATIONS**
- 10. NEW BUSINESS**
- 10.1 RDN Parks Funding Service Review 69**
- 11. IN CAMERA**
- That pursuant to Section(s) 90 (1) (e), of the *Community Charter* the Committee proceed to an In Camera meeting for discussions related to the acquisition, disposition or expropriation of land or improvements.
- 12. ADJOURNMENT**

REGIONAL DISTRICT OF NANAIMO
MINUTES OF THE REGIONAL PARKS AND TRAILS SELECT COMMITTEE MEETING

Tuesday, October 9, 2018
12:00 P.M.
Committee Room

In Attendance:	Director H. Houle	Chair
	Director A. McPherson	Electoral Area A
	Director M. Young	Electoral Area C
	Director B. Rogers	Electoral Area E
	Director J. Fell	Electoral Area F
	Director J. Stanhope	Electoral Area G
	Director B. Veenhof	Electoral Area H
	Director G. Fuller	City of Nanaimo
	Director I. Thorpe	City of Nanaimo
	Director M. Lefebvre	City of Parksville
	Director B. Colclough	District of Lantzville
Regrets:	Director J. Hong	City of Nanaimo
	Director B. Yoachim	City of Nanaimo
	Director T. Westbroek	Town of Qualicum Beach
Also in Attendance:	P. Carlyle	Chief Administrative Officer
	T. Osborne	Gen. Mgr. Recreation & Park Services
	W. Marshall	Mgr. Park Services
	A. Harvey	Recording Secretary

CALL TO ORDER

The Chair called the meeting to order and respectfully acknowledged the Coast Salish Nations on whose traditional territory the meeting took place.

APPROVAL OF THE AGENDA

It was moved and seconded that the agenda be approved as presented.

CARRIED UNANIMOUSLY

ADOPTION OF MINUTES

Regional Parks and Trails Select Committee Meeting - May 15, 2018

It was moved and seconded that the minutes of the Regional Parks and Trails Select Committee meeting held May 15, 2018, be adopted.

CARRIED UNANIMOUSLY

CORRESPONDENCE

T. Osborne spoke to the correspondence items.

It was moved and seconded that the following correspondence be received:

R. Robinson, Friends of French Creek Conservation Society, re: French Creek Estuary Lands

T. Cameron, Gabriola Resident, re: Accessibility of Descanso Bay Regional Park

D. James, Qualicum Beach Streamkeepers Society, re: Little Qualicum River Estuary

L. Lemon and P. Cuttris, Benson View Rd. Residents, re: Mt. Benson Regional Park Parking

CARRIED UNANIMOUSLY

UNFINISHED BUSINESS

Mount Benson Regional Park – Parking Lot Project Update

It was moved and seconded that following completion of the new parking lot, the existing parking lot and floating boardwalk at Witchcraft Lake remain open for one year and that a subsequent report to the Board be prepared on their use.

Opposed (5): Director McPherson, Director Fell, Director Fuller, Director Thorpe, and Director Colclough

CARRIED (6 to 5)

REPORTS

Parks Update Report – Summer 2018

It was moved and seconded that the Parks Update - Summer 2018 be received as information.

CARRIED UNANIMOUSLY

Morden Colliery Regional Trail Update

It was moved and seconded that the Morden Colliery Regional Trail Update be received for information.

CARRIED UNANIMOUSLY

Big Qualicum River to Alberni-Clayoquot Regional District Regional Trail Update

It was moved and seconded that the Big Qualicum River to Alberni-Clayoquot Regional District Regional Trail Update be received.

CARRIED UNANIMOUSLY

Trans Canada Trail – Renewal of Island Timberlands Licence

It was moved and seconded that the Regional District of Nanaimo enter into a licence renewal with Island Timberlands' for the Trans Canada Trail from August 1, 2018 to July 31, 2020.

CARRIED UNANIMOUSLY

Trans Canada Trail – Private Land Use Agreement

It was moved and seconded that the Regional District of Nanaimo 2018 - 2020 land use agreement with Gayle and Peter Brase for the Trans Canada Trail be approved.

CARRIED UNANIMOUSLY

Benson Creek Falls Regional Park – Access Improvements

It was moved and seconded that detailed design and planning for the truss bridge across Benson Creek, including associated trail improvements, proceed.

CARRIED UNANIMOUSLY

It was moved and seconded that detailed design and planning for a combination of stairs and new trail to the base of Ammonite Falls proceed.

CARRIED UNANIMOUSLY

ADJOURNMENT

It was moved and seconded that the meeting be adjourned.

CARRIED UNANIMOUSLY

TIME: 12:45PM

CHAIR

December 06, 2018

Mr Tom Osborne, General Manager, Recreation & Parks
830 West Island Highway
Parksville, BC, V9P 2X4
tosborne@rdn.bc.ca

French Creek Estuary Land

Dear Mr Osborne:

We would like to take this opportunity to introduce a community campaign, Save French Creek Estuary Land, representing citizens from French Creek, Parksville, Qualicum Beach, Nanaimo and many neighbouring communities and cities on Vancouver Island.

Our goals are to:

- Protect this critical area from a proposed development.
- Keep the land as natural space and wildlife habitat.
- Work together with the Regional District of Nanaimo (RDN), Conservancy agencies and the Community, to add the land to the existing Regional Community Park.

The acquisition of 23 acres of land bordering the west side of French Creek Estuary provides a wonderful opportunity for the RDN to protect sensitive ecosystems, support community partnerships, enhance the health and well-being of residents, and meet the growing public desire for natural spaces.

French Creek Estuary Land is recognized as a priority for conservation. This unique transect connects 2nd growth Coastal Douglas fir forest, estuary, river and Salish Sea, providing habitat for an abundance of wildlife. Among its inhabitants are raptors such as eagles, hawks and owls, as well as deer, otter, mink, beaver, turtles and a multitude of resident and migrating birds. Two of Vancouver Island's three, large carnivores, black bear and cougar, occur here. Some species are listed as at risk.

The RDN prioritizes protecting sensitive ecosystems, reducing harm to the environment, and enhancing the well-being of the community in its Regional Growth Strategy, Official Community Plan Area 'G', Community Parks & Trails Strategic Plan, and Regional Parks & Trails Plan. The OCP documents a 'strong community desire to preserve the large tract of forested land remaining between the Englishman River and Little Qualicum estuaries,' referring to this 23 acres of land.

Over 2000 petition signatures from residents of the RDN and neighbouring communities reflects strong, community support for the protection of this 23 acres as natural land and wildlife habitat.

We respectfully suggest collaboration between the RDN, Conservation Agencies, and the Community, to work in partnership to protect and acquire this ecological treasure. We invite you to visit our website to discover more about this unique area and enjoy amazing wildlife photos taken at the site.

Yours truly,

Denise Foster, Peter Giesen, Penny Vanson, Peter Bonell
On Behalf of Save French Creek Estuary Land
Website: savefrenchcreekestuaryland.ca Email: saveestuaryland@gmail.com

From: Friends of French Creek Conservation Society (FFCCS)

Estuary Lands Committee, Robin Robinson, Chair

Updated January, 12, 2019

Report on The French Creek Estuary Lands

The fate of the estuary lands at French Creek has been of prime concern for the Friends of French Creek Conservation Society since its inception in 2004. The following report was first compiled in September 2018 to give information of our progress in preserving this important and sensitive property. Like many land conservation projects, it is long slow work.

Most of the undeveloped land near the estuary is owned by French Creek Estates Ltd., with small sections of parkland owned by the Regional District of Nanaimo. Recently, less than half of the 23 acres, located between Hwy 19A, Columbia Drive, Viking Way and French Creek, has been contracted to be sold to a development company. The more sensitive area adjacent to the estuary is still owned by French Creek Estates Ltd. and is presently being proposed by them as a protected estuary park. The FFCCS Estuary Lands Committee is acting as the facilitator for this exciting prospect.

Some History

In 2006 the Nature Trust of BC, on behalf of FFCCS, held unsuccessful negotiations with French Creek Estates Ltd. owner, the late John Moore. Mr. Moore passed away in 2015, and during the summer of 2016 the 23 acres were put on the market with an asking price of \$6.9M.

In 2016, FFCCS established the Estuary Lands Committee, with the addition of some members from the French Creek Residents Association (FCRA) Board. The goal of the committee was to investigate feasible methods to secure as much of the estuary lands as possible for natural space and park, instead of for development.

The Committee has always been respectful of the fact that the land is privately owned with clear title and an RDN Development Permit (DP) dating from 1994. Fortunately, that DP has morphed over time with changes to BC's environmental standards and there is no longer a threat of multi-storey condominiums being built on the land.

Some results of the Committee's work to date:

- We established a productive working relationship with the owner of French Creek Estates Ltd. As a result, on Dec 11, 2018, FFCCS and FCRA signed a Memorandum of Understanding with

French Creek Estates Ltd. And French Creek House Ltd., the owners of the land This will facilitate the transfer of approximately 14 acres of the 23 acres into preserved parkland.

- We met with Ducks Unlimited Canada and the West Coast Conservation Land Management Program last fall and are presently discussing options for the management of the parkland.
- We compiled and completed an on-line Community Survey conducted in July, 2018. The response was overwhelmingly positive towards forming an estuary park. See our web page <http://www.ffccs.ca> for more details.
- During the summer of 2018, we worked in conjunction with the Mount Arrowsmith Biosphere Region Research Institute (MABRRI) and Vancouver Island University (VIU). VIU students and MABRRI staff conducted a land use and environmental survey of the estuary lands, and in October, 2018, a report *French Creek Estuary: Assessment of its Historical, Community, and Ecological Values* was published by MABBRI. The link to the MABRRI/VIU report is:

http://mabrri.viu.ca/sites/default/files/frenchcreek_analysis.pdf

In Conclusion:

FFCCS has had the goal of securing the estuary lands for almost 15 years. Signing an MOU with the landowner to protect approximately 14 of the 23 acres is an important step towards succeeding in

h

a

v

i

n

g

t

h

i

s

l

a

n

d

p

Memorandum of Understanding

Between:

FRENCH CREEK HOUSE LTD. (FCH) & ITS SUBSIDIARY
FRENCH CREEK ESTATES LTD. (FCE)

Registered British Columbia Companies with their offices at:
600 – 1090 West Georgia St.
Vancouver BC,

As represented by:
Quinn Griesdale, Director
French Creek House Ltd.
1235 E13th Ave.,
Vancouver, BC
(778) 791-1836

And:

FRIENDS OF FRENCH CREEK CONSERVATION SOCIETY (FFCCS)
Registered Association within the province of British Columbia

As represented by:
Patrick Weber
557 Viking Way, Parksville, BC
V9P 2Z5

&

FRENCH CREEK RESIDENTS ASSOCIATION (FCRA)
Registered Association within the province of British Columbia

As represented by:
Robert B. Williams
794 Mulholland Drive
Parksville, BC
V9P 2K4

This memorandum of understanding will be in effect once it has been ratified by resolutions by the Boards of Directors of French Creek Estates Ltd., French Creek Residents Association and Friends of French Creek Conservation Society.

Background:

French Creek House (FCH) and French Creek Estates (FCE) are the owners of several parcels of land bordered by Lee Rd. on the East, French Creek on the West, BC Highway 19A on the South, French Creek Estuary to the North and the French Creek Marina on the North East. The two corporations plan to apply for zoning on their lands to accommodate the development of a community involving residential, commercial and offices which will form as a hub for businesses which will provide services and residential accommodation to the residents of the area. Their plan involves the development of walking and cycling trails linking the communities on the east side of BC Highway 19A.

French Creek Estates (FCE) is the owner of 23 acres of land bordered by French Creek on the East, Columbia Drive on the West, BC Highway 19A on the South and the subdivided properties occupied by homes facing onto Viking Way and Admiral Tryon Blvd. to the North, and legally described as Lots 2-5 Plan VIP62528. These properties form a part of Development Permit 77 issued by the Regional District of Nanaimo in 1994 for clustered high density residential housing.

For several years the residents of the community have used these lands, without authorization, as green space and have developed an informal group of trails and bicycle paths on the property. Registered in the Regional District of Nanaimo Official Community Plan of 2007 is the communities desire to maintain this space as public and preserved green space. The French Creek Residents Association (FCRA) and the Friends of French Creek Conservation Society (FFCCS) have, over the years expressed an interest in purchasing this land and have diligently worked to protect the estuary, estuary lands and low lying wet lands.

French Creek Estates has entered into an agreement with Elkay Developments for the development of a part of these lands for residential purposes and are in the process of identifying the lands which will be separated from these parcels for development. The lands proposed for development are shown in white and light green on attached appendix "A" as lots A & B. Proposed development Lot A (light green area) has an approximate area of 0.78 hectares and Lot B (off-white area) would have an approximate area of 2.67 hectares (0.39 plus 2.28 hectares).

French Creek Estates is proposing that the remainder of the land (as shown in dark green on Appendix A) will be set aside as park land in order to protect the estuary, estuary lands and wet lands associated with French Creek. The proposed park land would include the remainders of lots 2, 3, 4 and 5 (minus lots A and B), plus the 0.14 ha. shown as being a part of lot 2 and the 0.41 ha. shown as being a part of lot 3 on Appendix B. A legal survey to clearly define the park lands will be completed as Lots A and B are removed for development.

French Creek Estates, French Creek House, French Creek Residents Association and the Friends of French Creek Conservation Society will work together as partners to identify the entity that will take ownership and the process for the ongoing management and maintenance of the park, its trails and creek crossings, in accordance with this memorandum of understanding.

Appendix A



Appendix B



Memorandum of Understanding

- A. Immediately following receipt of the development permits by Elkay Developments and the completion of their contract for purchase and sale with French Creek Estates and the re-zoning and development applications being in place for French Creek House, the transfer of the park lands will occur.
- B. Responsibility for maintenance, liability insurance and future upgrades will transfer with the ownership of the land or be delegated to another organization other than French Creek House. The responsibility for the trails in front of Estuary Estates properties will be established as the development proceeds in accordance with the requirements of the Regional District of Nanaimo and liability will remain with French Creek House
- C. An appropriate organization shall be selected, by mutual agreement of the parties to this agreement, to take ownership of the land. This organisation must be supportive of the use as described in this agreement and may be comprised of a single entity or a partnership comprised of two or more government and non-government organizations in order to provide both the ability to hold title to the land and to provide ongoing operation and maintenance of the trails and infrastructure.
- D. The organization as described in clause C will establish a project to develop pedestrian and bicycle trails and bridges based on the advice of environmental consultants, and the studies conducted by Vancouver Island University and the Mount Arrowsmith Biosphere Regional Research Institute. All of the parties to this memorandum of understanding will work to identify and obtain funding for the project through grants, donations, government funding or programs or any combination thereof.
- E. The primary trails included in the project (shown as charcoal on appendix C), are approximately 600m in length starting at Viking Way and will create a contiguous connection crossing both the proposed park land and the lands proposed to be developed by French Creek House and continue to a bridge proposed to be constructed at Morningstar Creek (shown on Appendix C). This trail should be a surface suitable for pedestrians, bicycles, wheel chairs and other mobility aides.
- F. The project will also include a bridge with a span of approximately 45m. at the proposed location shown on Appendix C. This location was chosen as it represents the shortest span requirement. It is understood that the location of proposed bridge would be based on satisfying all environmental requirements and the location may change to reflect this.
- G. A project manager will be appointed by the partnership to oversee the trails and bridge projects.
 - a. Project reporting must be made to all parties involved on a quarterly basis or on written request by the representatives of the parties.
- H. Trail and bridge design must be approved by all parties involved before proceeding with construction.

I. Construction requirements:

- a. The Primary trails and bridge surfaces are to be designed and constructed for ease of use of disabled persons
- b. Secondary trails and look outs will be designed and built to reduce the environmental foot print and preserve sensitive areas
- c. The design and costs for the construction of trails and the bridge are to be mutually agreed upon by all of the parties
- d. Other requirements may result through discussions with the Regional District of Nanaimo, agencies of the Province of British Columbia or the corporate entity that will become the owners of the park land.

J. Branding and marketing of the name "French Creek Trails" will remain the property of French Creek House until June 30th, 2028 or the completion of the French Creek House development, also know as French Creek Village. French Creek House will authorize the use of the name for the trails and/or park land and will work with the partnership on any marketing of the park land and trails.

K. All parties to this agreement will work together to develop a mutually beneficial public image of the park, trails, bridge and the development of the community.

L. It is understood and agreed by all of the parties to this memorandum of understanding that this agreement will apply to the current parties to the agreement and all heirs or assigns and, once the surveys are completed to identify those lands which will be removed for development, this memorandum of understanding will either be registered as a covenant against the title or titles of the lands to be dedicated as park land or will be used to develop such a covenant.

IN WITNESS WHEREOF the parties hereto have executed this Agreement as of the 11th day of December, 2018.

FRENCH CREEK HOUSE LTD.

By: 
Authorized Signatory

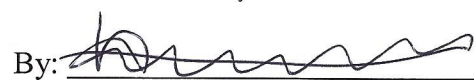
FRENCH CREEK ESTATES LTD.

By: 
Authorized Signatory

FRIENDS OF FRENCH CREEK
CONSERVATION SOCIETY

By: 
Authorized Signatory

FRENCH CREEK RESIDENTS
ASSOCIATION

By: 
Authorized Signatory

Appendix C



Motion to participate in a Memorandum of Understanding by the Board of Directors of the French Creek Residents Association:

Moved - that the French Creek Residents Association (FCRA) enter into a Memorandum of Understanding (MOU) concerning French Creek Estuary Lands between the owners French Creek House Ltd (FCH) and it's subsidiary French Creek Estates Ltd (FCE) together with the Friends of French Creek Conservation Society (FFCCS) and FCRA for the purpose of acquiring those lands to be placed in trust with an entity yet to be determined. Further, the FCRA Board of Directors understands that in signing this MOU, the Association is not acquiring any financial obligation that would encumber the Association or it's Members. It is further understood that FFCCS has obtained Independent Legal Advice (ILA) to confirm there are no financial obligations for either Association.

Moved - Williams

Seconded - Swannack

Carried

Date - December 6, 2018

Motion to authorize a Representative to sign the MOU on behalf of the Association:

Moved - President Rob Williams be authorized to sign the MOU between French Creek Estates, French Creek House, FCRA and FFCCS when finalized.

Moved - Coath

Second - Shaw

Carried

Date - November 1, 2018

The above motions by the Board of Directors are certified to be as recorded in Minutes for the Association on the date indicated:


Michael Jensen
Secretary
French Creek Residents Association

Date 

Friends of French Creek Conservation Society
Special Board of Directors Meeting
Held November 1, 2018
484 Columbia Drive, Parksville, BC

Present: Ray Smith (chair), Sunshine Goldsberry, Liberty Bradshaw, Rosemary Ashworth, Brian Coath and Sandy Robinson.

Guests: Robin Robinson, Patrick Weber

The following motions were passed:

Motion 1:

Moved by Sandy Robinson, seconded by Brian Coath, that:

The Friends of French Creek Conservation Society Board of Directors has reviewed the Memorandum of Understanding between French Creek Estates, French Creek House, French Creek Residents Association and Friends of French Creek Conservation Society and approve the signing of the Memorandum of Understanding once independent legal advice has been received confirming that there are no financial obligations that would impact the Friends of French Creek Conservation Society membership or its Board of Directors; and that signing the Memorandum of Understanding will not pre-empt any opportunity to obtain the balance of the property should the opportunity arise.

Motion 2:

Moved by Liberty Bradshaw, seconded by Rosemary Ashworth, that:

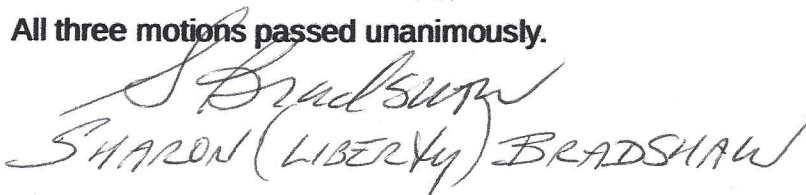
the Friends of French Creek Conservation Society Board of Directors authorize Patrick Weber to sign the Memorandum of Understanding as its representative.

Motion 3:

Moved by Sunshine Goldsberry, seconded by Sandy Robinson, that:

the Friends of French Creek Conservation Society Board of Directors delegates signing authority for the purposes of execution of the Memorandum of Understanding to Patrick Weber, limited to this action only;

All three motions passed unanimously.


SHARON (LIBERTY) BRADSHAW

TO: B POSAC, EW/PV POSAC, E POSAC, F POSAC,
G POSAC, H POSAC, EA A Parks, Recreation and
Culture Commission, D69 Recreation Commission,
Regional Parks and Trails Select Committee

MEETING: January 28, 2019

FROM: Wendy Marshall
Manager of Parks Services

SUBJECT: Parks Update Report. – July - September 2018

RECOMMENDATION

That the Parks Update Report - July - September 2018 be received as information.

SUMMARY

Parks staff continue to work on projects identified in the 2018 Parks Work Plan and the RDN Operational Plan.

BACKGROUND

A summary of key projects undertaken between July and September is provided below. The attached spreadsheet provides details on park projects.

Planning and Capital Projects – Key Highlights

707 Parkland Additions

The Gabriola Island Local Trust Committee has approved Bylaws 289 and 290, amending the Gabriola Official Community Plan and Land Use Bylaw and allow the transfer of 136 hectares of parkland adjacent to the 707 Community Park to the Regional District of Nanaimo (RDN) in exchange for the creation of up to 25 lots on the properties between Cox Community Park and the Village Core. The new parkland is located between 707 Community Park and Coats Marsh Regional Park. With this transfer, 707 Community Park on Gabriola Island will increase from 707 acres to 1,043 acres, providing increased ecological protection to Coats Marsh and important trail connections for the island. Until the 707 Community Park Management Plan is updated, the new parkland will be managed as an extension of the current park management plan. In the short term, the RDN has set aside funding to provide basic park amenities such as signage and park access gates.

Mount Benson Parking Facilities RP-S4-1.20

The design of the new parking lot and trail head for Mount Benson Regional Park is nearing completion and the project is expected to go to tender in November 2018. Construction would begin in the late-winter/early-spring 2019, with the goal of having the site open for summer 2019.

Huxley Park Projects RP-S4-1.2

The design consultant continues to work on developing construction drawings and costing for the Gabriola Skatepark to be completed in 2018. A development Variance Permit application will be submitted to the Islands Trust this fall requesting a relaxation on side-yard setbacks for the skatepark. Staff continue to provide support to the *Keep on Pushin'* community fundraising campaign to raise \$72,000 towards the construction of the facility

Big Qualicum River to ACRD Regional Trail RP-S4-1.12

The Victoria Land Titles Deputy Registrar recommends creation of a right of way plan to dedicate the 1911 Gazetted Horne Lake and Alberni Road, with the Ministry of Transportation and Infrastructure (MoTI) and Island Timberlands signing off. Once established, the titled road corridor will be available for the regional trail. MoTI supports the proposal and discussions will be held with Island Timberlands to resolve final issues surrounding road location and pave the way for survey work to create and register a plan of dedication. A request for qualifications will be issued to identify potential design-builders of the trail. Planning meetings will continue with ACRD as well additional meeting with the First Nation communities within the ACRD and the RDN areas will be arranged to review and confirm the trail's location.

Coombs to Parksville Rail Trail

Work has been completed this past October to repair the section of trail flooded in January 2018. The repairs include raising the trail and installation of additional piping and a manhole. The repair area involved fish bearing Morningside Creek requiring Ministry of Forests, Lands and Natural Resource Operations Section 11 approval before works could begin. Island Corridor Foundation approval was also obtained for trail redesign intended to mitigate the risk of future inundation. Concerns about upstream land development and related surface drainage as well as MoTI maintenance of Hwy 4A culvert and ditch around Morningside Creek have been relayed to the Ministry and to RDN Bylaw Services for action.

Village Way Path

Revised preliminary plans have been submitted to MoTI for review. If MoTI accepts the proposed solution to road drainage, detailed plans will be completed and costed, information meetings will be arranged with landowners fronting the North Road path, and a public open house will be scheduled to bring the community up to date.

Benson Creek Falls Facilities RP-S2-3.7

A report recommending that the project proceed with detailed design of a recreational truss bridge across Benson Creek and improved access to Ammonite Falls by way of a stairs and trail combination was reviewed by the Regional Parks and Trails Select Committee in October. The recommendation has been forwarded to the RDN Board for approval. Detailed design would occur in 2019 with construction to follow as early as 2020.

Meadowood Way Community Park – Community Centre Development RP-S3-1.7

The Detailed Design and Costing have been completed by Herold Engineering Ltd. and presented to the Electoral Area Director and the Corcan Meadowood Residents Association (CMRA). An Operation Agreement is underway for the CMRA to run the facility once constructed. A board report is to follow.

Blue Water Community Park Restoration

Two rounds of vegetation management have been completed (spring and fall 2018). Goats were used to clear the park of invasive plants. The trees have been planted, funded by a BC Hydro grant, and revegetation seed mix has been provided in targeted areas in the park. Further vegetation management and restoration efforts are planned for 2019.

Driftwood Beach Access Development RP-S4-1.1

Installation of a culvert is complete and stair construction is underway to improve access and provide a safe descent to the beach. The project is permitted through the MoTI. Construction will be completed this fall.

Little Qualicum Hall Renovation RP-S2-3.6

Initial renovation plans were completed in-house and have been presented to representatives of the neighborhood association. Herold Engineering Ltd. is currently working on the detailed design and will be acting as the Coordinating Registered Professional of Record for this project. Permit application is expected this fall with construction to commence early in 2019.

Dunsmuir Community Park Development RP-S4-1.7

Dunsmuir Phase 1 construction is underway and is expected to be completed in December.

Registry and Mapping

GIS has created web map acquisition layers for parks, trails and water accesses in Areas C, E and F, and individual acquisition maps for Areas C and E, paving the way for the creation of property registration forms as completed for Areas A and B. Area E Planning Map has been created, which will reflect a recent Fairwinds development addition to that Area's portfolio. Various RDN permitted water accesses that had never been GPSed are now captured in the web map; missing MoTI water access permits have been obtained. The Parks and Trails Finder was updated and edited.

Operations – Key Highlights

Service Software RP-R2-4.6

New service software is now in use for Parks Operations staff. The program schedules inspections, provides digital input screens for use in the field, creates follow up tasks for maintenance issues found during inspections and provides documentation of inspections. Staff can create work orders for any task including requests from residents. These requests will now be documented and follow up actions recorded. The software also creates reports summarizing the number of inspections, resident calls and site visits that were completed during a given time frame.

General Service Calls

- 508 documented inspections were completed over 147 sites, resulting in 48 work-order items for follow-up actions or repairs.
- Sixty resident requests were received including: Tree complaints, vegetation management requests, dumping and garbage complaints, animal related complaints, graffiti, vandalism, and signage requests.
- Operations staff routinely provide support to the delivery of many ongoing projects and improvements throughout the park system in addition to their regular maintenance tasks.

Parks Programming

Nine programs, both new and returning, were offered in parks this fall. Two sessions of the Amazing Race, a Hiking Information Session (free for Active Aging Week), Wesley Ridge Hike, Geology Tour of Nanoose Bay (including stops at Beachcomber Regional Park and Blueback Community Park), and Salmon Tour (at Big Qualicum River Regional Trail) all had or will have enough registration to run. Paddling Adventures at Horne Lake and the Mt. Horne Hike were cancelled due to low registration and a Stand Up Paddleboard program was cancelled due to weather.

Park Use Permits

Area A – A (non-exclusive) permit was granted to Cedar Elementary School (SD 68) to use Addison Way Community Park for an outdoor learning space. In addition, a permit was granted to Cedar Elementary and First Neck Point Scout group to do volunteer cleanup (removal of garbage and some debris) from Addison Way Community Park to prepare for use by the school.

Area B – Gabriola Softball Association was granted a permit to run a softball tournament September 22-23, 2018.

Area C – No permits this quarter.

Area E – A permit was granted for a wedding (<50 people) at Beachcomber Regional Park September 15. Nanoose Bay Elementary School (SD69) was granted permits to visit Moorecroft Regional Park as an outdoor learning space, and for use of Nanoose Road Community Park for their Mountain Bike Club to access trails. VIU Education Program was granted a permit to bring student teachers to Moorecroft Regional Park to teach sustainability and outdoor education.

Area F - Corcan Residents Association was granted a permit for their 8th Annual Halloween event, including fireworks, at Meadowood Community Park.

Area G - No permits this quarter.

Area H - No permits this quarter.

FINANCIAL IMPLICATIONS

The projects outlined in this report have funds identified in the 2018 Budget. Electoral Area projects are funded through the associated 2018 Community Parks Budget and in some cases are supplemented by Community Works Funds or grant funding. Regional Parks projects are funded through the 2018 Regional Parks Operational Budget or the Regional Parks Capital Budget.

STRATEGIC PLAN IMPLICATIONS

Projects in this report support the RDN's strategic priorities for Service and Organizational Excellence and Focus on the Environment:

- Delivering efficient, effective and economically viable services that meet the needs of the Region;
- We will fund infrastructure in support of our core services employing an asset management focus;
- As we invest in regional services we look at both costs and benefits — the RDN will be effective and efficient;
- We recognize community mobility and recreational amenities as core services; and
- We will have a strong focus on protecting and enhancing our environment in all decisions.



Wendy Marshall
wmarshall@rdn.bc.ca
December 3, 2018

Reviewed by:

- T. Osborne, General Manager, Recreation and Parks
- P. Carlyle, Chief Administrative Officer

Attachments:

1. Parks Work Plan – Fall 2018

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
A	16-785	2017-002	Beach Accesses	Driftwood Beach Access consultation and design	2017	2018 Q3	Completed	Board report completed and design to move forward during summer. Design completed.
A	18-149	2018-001	Beach Accesses	Driftwood Beach Access stairs construction	2018	2018 Q2	Underway	Construction underway to be completed this fall.
A	17-153	2018-025	Cedar Plaza	Review Possible Alternatives to allow water	2017	2018 Q2	Underway	Application into NCID waiting for approval.
A	18-224	2018-074	Cedar Plaza	Installation of a communications board			Not started	TBD
A	18-144	2018-026	Glynneath	Trees - removal/replant. Keep park in natural state	2018	2019 Q2	Completed	Harvesting Assessment complete, 2018 planned work complete.
A		2018-024	Kipp Road	Geotechnical options for site remediation	2018	2018 Q2	Underway	Remediation technical plan complete and cost estimate complete. Funding requested to carry out work in 2019
A	17-516	2018-023	Skatepark	Review options for a viewing platform	2018	2019Q3	Not started	Identified in 5-year work plan.
A	18-223	2018-075	Thelma Griffiths	Pilot Park for new signs	2019		Not started	Planned for 2019
B	16-348, 18-008	2017-006	707	New Signs as per plan	2018	2017 Q4	On hold	Delayed until parkland addition brought into the RDN system.
B		2018-027	707	Gate, drainage and road repairs	2018	2018 Q3	Underway	Fire Department Access Improved , road and drainage repairs underway.
B		2018-028	707	New Property - add amenities	2018		Not Started	Will start once property added to the RDN system.
B			707	Management Plan Update	2020		Not Started	Planned for 2020.
B			707	New Property development	2023		Not Started	Planned for 2023.
B		2018-031	Cox	New Property - add safety amenities	2018		Not started	Will start once property added to the RDN system.
B			Cox	Master plan	2021		Not started	Planned for 2021.
B			Cox	Property Development	2022		Not started	Planned for 2022.
B	18-179		Cox	Work with Galtt on trails	2018	2018 Q3	Completed	Staff working with Galtt on trail layout and marking. Completed
B		2018-029	Huxley	Electrical Upgrades Assessment	2018	2018 Q3	Underway	Consultant hired. To be completed by end of Nov.
B			Huxley	Grant applications for Skate Park	2019		Not Started	Any grant opportunities will be examined.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
B			Huxley	Skatepark Construction	2020		Not Started	Planned for 2020.
B		2018-061	Huxley	Park Opening Event	2018		Completed	Opening event held.
B	18-182	2018-076	Huxley	Pickle ball line painting	2018	2018 Q4	On Hold	On hold due to contractor availability/weather in 2019. To be completed summer 2019.
B	16-346	2016-007	McCollum Road Cash in Lieu	Work with Developer for processing the Cash in Lieu	2016	TBD	Underway	Waiting to received payment from developer.
B		2018-033	Pilot Bay and Huxley Benches	Benches	2018	2018 Q3	Underway	Huxley Benches Installed.
B		2018-032	Whalebone	Upgrades to trails	2018	2018 Q2	Completed	Upgrades complete.
B-CWrks	18-180	2018-030	Huxley	Skateboard and park entrance Construction drawings/costing/geotec/survey	2018	2018 Q2	Underway	Consultant hired. To be completed by end of Dec. Geotech underway.
B-CWrks	Staff	2017-009	Joyce Lockwood Stairs	Rebuild Beach Access stairs	2018	2018 Q3	Suspended	Unfavourable Geotech Report. Replacement will not proceed.
B-CWrks	Board	2015-001	Village Pathway	Design/MOTI approval	2015		Underway	Met w MOTI 18May; identified possible approach. Project engineer contracted to revise plans and confirm feasibility.
B-CWrks		2017-011	Village Way Path	Construction	2019	2020	On hold	On hold until MOTI approval of design is obtained.
C EW/PV	Posac	2018-003	Anders Dorrit	Information sign design and install	2018	2019 Q3	Not started	Planned for 2019.
C EW/PV	Posac	2018-004	Anders Dorrit	Concept Design & Open House	2018	2018 Q3	Completed	Open house held at end of April.
C EW/PV			Anders Dorrit	ALC, MOTI approvals	2018	2019-Q2	Underway	Application to ALC submitted. Awaiting response.
C EW/PV			Anders Dorrit	Detailed Design	2019	2019 Q3	Not started	To follow permits and approvals.
C EW/PV			Anders Dorrit	Tender & Construction	2020	2021	Not started	Planned for 2020.
C Ext		2018-035	Extension Miners	Coal artifact display, trail to river design	2018	2018 Q4	Not started	Now planned for 2019.
C Ext			Extension Miners	Install Trail to river	2019		Not started	Planned for 2019.
C Ext		2018-034	Trails Initiative	Horse Access on Crown	2018	2018	Underway	Coordinating with Backcountry Horsemen of BC rep.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
E	18-084	2018-055	Bonnington Drive/Coventry Place	High level report on cost and process for stairs and trail construction	2018	2018 Q3	Completed	Report presented at fall POSAC.
E	18-085	2018-037	Brickyard	Parking, Toilet, Garbage - Concept Layout & Neighbour Consultation	2018	2018 Q2	Suspended	Suspended as per board direction.
E	18-085	2018-038	Brickyard	Toilet, Garbage Install	2018	2018 Q3	Suspended	Suspended as per board direction.
E	16-197	2016-009	Claudet	Utilities lot transfer	2016	TBD	Underway	Staff reviewing approach to dispose of parkland.
E			Claudet	Master Plan	2020		Not started	Added to five year plan as per POSAC request.
E		2018-039	Es-hw Sme~nts	Interpretive signs -design and install	2018	2018	Completed	Completed.
E	18-087	2018-083	Jack Bagley	Court placement and design	2019		Not started	Survey to take place in 2018 and planning in 2019.
E	17-155, 17-409	2018-036	Nanoose Road Community Park	Lease renewal, public consultation, Design new use once received	2018	2020	Not started	Option to purchase to be considered.
E	17-517, 18-086	2018-074	Stone Lake Drive	Natural Playground Site survey, Public Input & Preliminary Design	2018	2018 Q3	Underway	Survey completed and public consultation underway.
			Water Accesses	Inventory by POSAC Sub-Committee	2017	2018 Q4	Underway	Spreadsheet completed. Staff to support report compilation.
E			Water Accesses	Priority from Study (sign some existing access points)	2019	2019 Q4	Not started	Planned for 2019.
E			Water Accesses	Priority from Study (survey, design, install)	2020		Not started	Planned for 2020.
F	17-310	2018-009	ACT Trails	Surface David Lundine trail	2018	2018 Q2	Underway	Trail construction underway. Will be completed by the end of October.
F		2018-040	ACT Trails	Palmer Road East Trail Construction	2018	2018 Q4	Underway	Topo survey underway.
F			ACT Trails	Bellevue Trail 1 - Grafton/Bellevue	2019		Not started	Planned for 2019.
F			ACT Trails	Bellevue Trail 2 - Koperick link	2019		Not started	Planned for 2019.
F	17-312	2018-006	Errington	Park Master Plan to include a bike skills park and a playground	2017	2019 Q3	Underway	Final Concept Master Plan underway. Survey, Geotech rev, and costing complete.
F			Errington	Phase I detail design	2019		Not started	Planned for 2019.
F			Errington	Phase II Detail Design	2021		Not started	Planned for 2021.
F			Errington	Phase II Construction	2022		Not started	Planned for 2022.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
F	Staff	2018-007	Errington	Operator Agreement	2018	2018 Q4	Underway	Due to water/septic infrastructure staff investigating options including Crown grant.
F	18-183	2018-077	Malcolm	Pilot Park for new signs	2019		Not started	Planned for 2019.
F	18-196	2018-041	Meadowood	Community Hall Construction	2018		Underway	Schematic design and Class B estimate complete. Hall operator agreement drafting underway to be followed by a Board report.
F-CWrks	Posac	2019-001	Errington	Phase I construction	2020		Not started	Planned for 2020.
G		2018-056	Blue Water	Clearing and restoration.	2017	2020 Q3	Completed	Vegetation management and planting complete for 2018.
G		2017-015	Boulton	Replace playground borders	2017	2018	Completed	Work carried out by Ops Staff
G			Dashwood	Playground Updates	2019		Not started	Planned for 2019.
G		2018-057	Lee Rd	Tree removal French Creek	2018	2018 Q1	Completed	Trees removed from the creek.
G	18-185	2018-078	Maple Lane	Develop plan and add to five year plan	2018	2018 Q3	Completed	Added to five year plan.
G	18-184	2018-079	Maple Lane	Pilot Park for new signs	2018	2018 Q3	Underway	Signs under development
G	Posac	2018-010	River's Edge	Playground concept design	2019		Not started	Planned for 2019.
G			River's Edge	Playground detail construction drawings	2020		Not started	Planned for 2020.
G			River's Edge	Playground Install	2021		Not started	Planned for 2021.
G-CWrks	Posac	2019-002	French Creek Paths/Trails	TBD	2020		Not started	Planned for 2020.
G-CWrks	17-158, 17-405, 17-600	2018-011	Little Qualicum Hall	Engineering study and community consultation to determine direction	2018	2018 Q3	Completed	Engineering study complete and consultation underway with community. Report complete and direction determined.
G-Wrks	18-295	2018-084	Little Qualicum Hall	Undertake safety and accessibility upgrades	2018	2019 Q3	Underway	Concept plan complete and approved. Plans are with engineering consultant for permit drawings.
H		2018-042	Beach Accesses	Signage and Improvements	2018	2018 Q3	Underway	Beach Access priorities determined at June 11 POSAC. Signs will be installed in fall/winter.
H	17-020	2017-016	Dunsmuir	Detailed design	2018	2018 Q2	Completed	Phase 1 tender drawings completed and project tendered.
H			Dunsmuir	Phase II Planning	2019		Not started	Planned for 2019.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
H			Dunsmuir	Phase II Construction	2020		Not started	Planned for 2020.
H		2016-013	Lions Park	Operators Agreement	2016	2018 Q3	Underway	Revised licence sent to Lions 30Apr. Follow up meeting taken place and changes underway.
H	Posac	2019-003	Oakdowne	Licence on other crown parcels surrounding the park	2019		Not started	Planned for 2019.
H			Sunny Beach	Improvements	2019		Not started	Planned for 2019.
H	Posac	2018-013	Wildwood	Kiosk development and install - split with Regional	2018	2018	Underway	Artwork for sign is under review.
H	18-143	2018-080	Wildwood	Pilot Park for new signs	2018	2018 Q4	Not started	Planned for fall in conjunction with kiosk design.
H-CWrks	Other	2020-001	Area H Roadside Trails	Work with MOTI on any development of trails along the road.	2020		Not started	Planned for 2020.
H-CWrks	18-148, 18-147	2018-012	Dunsmuir	Phase I construction	2018	2018 Q3	Underway	Construction underway to be completed by end of November.
Other - Comm	Other	2018-014	Bike Network Plan	Develop plans	2019		Not started	Planned for 2019.
Other - Comm	Operational		Development	Subdivision/parkland - review, comments and POSAC input			On Going	Ongoing as required through planning.
Other - Comm	16-616, 18-061	2016-018	Park Signage	Resign Park regulation and info signs	2016	2018 Q3	Completed	Board and committees have reviewed and pilot parks selected.
Other - Comm	18-357,		Park Signage	Install new signs in pilot parks in each EA			Underway	First sign designs underway
REG	Staff	2017-019	Ammonite Falls Trail	Bridge Replace	2017	2019 Q3	Underway	Environmental Assessment complete. Design estimates underway, construction in 2019 pending funding.
REG	Staff	2018-020	Arboretum	Kiosk upgrade -design new sign with volunteers and install	2018	2018 Q4	Underway	Design of signage has started.
REG	Mngmnt Plan	2018-045	Beachcomber	Main Path Upgrades (stairs/handrails)	2018	2018 Q4	Completed	Handrail installed, trail surfacing and box stairs complete.
REG	18-072	2016-028	Beachcomber	Management Plan	2016	2017	Completed	Board approved plan.
REG	16-669	2017-048	Benson Creek Falls	Parking feasibility study and design - Weigles entrance (incl. env. study)	2017	2018 Q1	Completed	Concept planning complete. Approvals, detailed design to follow.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG	16-668	2017-049	Benson Creek Falls	Parking - monitor and communicate with residents	2017		Ongoing	Ongoing.
REG			Benson Creek Falls	Weigles entrance parking Lot Construction	2019		Not started	Will occur with stair and bridge construction.
REG	Mgmt. Plan	2018-019	Benson Creek Falls	Kiosk	2020	2020	Not started	Planned for 2020.
REG			Benson Regional Trail	Signage	2019	2020	Not started	Planned for 2019.
REG		2018-043	BigQ/ACRD Regional Trail	Detailed planning	2018		Not started	Will proceed once preliminary work is completed.
REG		2020-003	BigQ-ACRD Regional Trail	Construction	2019	2020	Not started	Would take place after detailed design.
REG		2018-048	Coats Marsh	Building and Bat housing	2018		Underway	Bat house installed. House removal by end of year.
REG	Staff	2018-018	Descanso	Tree removal	2018	2018 Q2	Underway	Coastal Wildfire carried out Phase 1, Phase 2 underway by contracted arborist to be completed in fall.
REG			Descanso	Gate house replacement	2020		Not started	Planned for 2020.
REG		2018-046	Englishman River	Geo technical study for the bank above the road	2018	2018 Q3	Underway	Geotech Consultant to site, awaiting on assessment proposal.
REG	Other		Fairwinds	Development - PDA		TBD	Underway	Staff have met with Planning Staff to review development proposal and PDA amendments.
REG	16-767	2017-024	First Nations Passage via Horne	Heritage designation	2017	2020	On Hold	Address once regional trail development has advanced.
REG			Horne Lake	Picnic shelter design/tender	2021		Not started	Planned for 2021.
REG		2018-044	Horne Lake and Descanso	Registration Software	2018	2018 Q2	Underway	Registration software purchased. Implementation underway.
REG		2017-021	La Selva Trail	Construction	2017		Delayed	Delayed due to land issue at site.
REG			Lighthouse	Nile Creek Bridge	2021		Not started	Planned for 2021.
REG			Lighthouse	Fletcher Creek	2023		Not started	Planned for 2023.
REG	16-480	2016-026	Moorecroft	Site Planning with First Nations (as per workshop priorities)	2016	2017 Q4	Ongoing	Consultation underway when required.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG	Mgmt. Plan	2018-021	Moorecroft	Picnic shelter design/tender	2020		Not started	Planned for 2020.
REG			Moorecroft	Parking Lot Design & Improvements	2021		Not started	Planned for 2021.
REG			Moorecroft	Meadow Road Upgrade	2019	2019 Q2	Not started	Planned for 2019.
REG			Moorecroft	Reroute Water Line	2019	2019 Q2	Not started	Planned for 2019.
REG			Moorecroft	Entry Area Improvements	2020		Not started	Planned for 2020.
REG		2018-049	Moorecroft	Miss Moore's Cabin Removal	2018	2018 Q3	Underway	Cabin removed. Planning for the former house site underway.
REG			Moorecroft	Interpretive Signage (design & install)	2021		Not started	Planned for 2021.
REG			Moorecroft	Kiosk	2020		Not started	Planned for 2020.
REG	Mgmt. Plan	2017-041	Moorecroft	Two vault Toilets & storage (design, locate, install)	2017	2018 Q3	Underway	Archeologist review of site complete, vaults installed, waiting for building installation.
REG	16-666, 17-510	2018-054	Mount Benson	Parking design and construction	2018	2019	Underway	House removal complete. MOTI permit received. Tender in fall 2018.
REG			Mount Benson	Trail Improvements - Condition and Evaluation of High Use Routes	2019		Not started	Planned of 2019.
REG			Olympic Torch Trail and Thames Crk Bridge	Planning	2020		Not started	Some reviews done for seniors housing project.
REG			PQ Links	Barclay Bridge - asphalt extension	2019		Not started	Planned for 2019.
REG	Staff	2017-020	TCT	Timberlands Road trail head development	2016	2018 Q4	Underway	Draft license for landowner and Island Timberlands awaiting Board approval. Detailed planning of parking lot completed. Rural Dividend Program updated.
REG		2018-080	TCT signage review	GPSing of signage in prep for updating given new TCT signs.	2018		Underway	Staff GPSing signs.
REG			TCT signage update	Apply for free signage if available; otherwise purchase. Update all.	2019		Not started	Planned for 2019.
REG	Other	2019-004	Top Bridge	Reroute trail and parking lot in City of Parksville	2021		Not started	Timing depends on the City of Parksville.
REG			Top Bridge	Connection to Rail Trail - Design	2019		Not started	Planned for 2019.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG			Top Bridge	Connection Construction	2020		Not started	Planned for 2020.
REG	Posac	2018-017	Wildwood/LHRT	Kiosk development and installation - split with H	2018	2018	Underway	Artwork for sign is under review.
REG	18-176	2017-070	Witchcraft Lake Regional Trail	Volunteer Trail Building Agreement	2017	2018 Q1	Completed	Agreement approved by Board and issued for signature.
REG	18-176	2018-047	Witchcraft Lake Regional Trail	Volunteer Trail Building	2018	2018 Q3	Underway	Agreement complete, logistical planning and material assembly underway, fire ban delayed start.
REG	18-177	2018-081	Witchcraft Lake	Costing and location for moving boardwalk	2018	2018 Q4	Underway	Working with City of Nanaimo on location and costs.
REG CAP		2018-050	Benson Creek Falls	Detailed design and costing stairs and bridge	2018	2018 Q4	Not started	Planned for late 2018. Awaiting resolution from Board to proceed.
REG CAP			Benson Creek Falls	Bridge Construction	2019	2019/2020	Not started	Planned for 2019.
REG CAP			Benson Creek Falls	Stairs at falls Construction	2019	2019/2020	Not started	Planned for 2019.
REG CAP	16-670	2017-031	Benson Creek Falls	Concept Plans and consultation Bridge and Stairs	2017	2018 Q4	Completed	Concept plans and public consultation completed. Report went to October RPTSC, forwarded to Board.
REG CAP		2017-043	BigQ/ACRD Regional Trail	Preliminary planning including agreement with strata	2017		Underway	Brief provided to Strata received with no comment. MoTI advises RDN to negotiate 1911 Horne Lake Rd plan revisions with Island Timberlands.
REG CAP	17-227	2017-073	E&N -- Coombs to Parksville Rail Trail	Horse Parking - Wood lot	2017		On Hold	Awaiting Springhill Rd development.
REG CAP		2018-062	E&N -- Coombs to Parksville Rail Trail	Trail Repair - Clean up, Design and rebuild		2018 Q3	Underway	Final design being detailed with input from environmental monitor; subject to approval by ICF and SVI. Building contractor lined up. Work to be completed by end of October.
REG CAP		2017-022	E&N - Coombs to Parksville Rail Trail	Finish Construction Details	2018		Underway	First review of as-built plans completed. Final plans await completion of flooding repairs.
REG CAP	17-386	2017-068	Little Qualicum	Bridge Design	2018	2019	Underway	Survey and geotech review underway. Detail Design and Costing to be complete by November.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG CAP			Little Qualicum	Bridge Construction	2020	2020 Q4	Not started	Planned for 2020.
REG CAP	Board	2014-001	Morden Colliery	Lease upgrade	2014		Underway	Staff working with Province on lease upgrade request.
REG CAP	16-124,14-755,14-754	2017-047	Morden Colliery	Bridge and trail design and tender	2017		Underway	ALC provided approval early July, with development conditions and July 2021 deadline for completion. Game plan being established.
REG CAP			Morden Colliery	Construction	2019	2020	Not started	Will commence after detailed designs are completed.
REG - Other	Staff	2018-053	Brochure	Design New Brochure/Print	2017	2018 Q3	Completed	Guides available in hardcopy or online.
REG - Other	Operational		Operator agreements	Monitor			Ongoing	Ongoing
REG - Other	Other	2018-016	Park Zoning	With Planning Department	2019		Not started	Planned for 2019.
REG - Other	Operational		Partnerships	Meetings and on-going communication with partners			Ongoing	Ongoing
REG - Other	Operational	2017-042	RPT Plan	Develop RFP. Work with consultant on plan development	2018	2019 Q4	Not started	RFP to be prepared during fall for Board report in the spring of 2019.
REG - Other	16-654, 16-678, 17-019, 18-030	2016-020	Salish Sea Marine Trail	Agreement with BC Marine Trail Association & signage	2016	2018 Q3	Completed	Signage received from BCMTNA and installed at five approved locations. DBRP pamphlet updated to reflect being on Salish Sea Marine Trail.
REG - Other		2017-072	Tourism VI Trails Strategy	Phase 3 Exceptional Hiking Experiences Network	2017		Underway	Met w TVI n VISTA to encourage TVI focus on this multi-region trail. Strong interest.
Other	Operational	2018-051	2019 Budget and Work plan	Create and forward for approvals	2018	2019 Q1	Underway	Budgeting for 2019 underway.
Other	18-217	2018-082	Wicklow West land exchange	Work with developer on land exchange	2018	2019 Q2	Underway	Subdivision applications submitted. Working on process for disposition of parkland.
Other			Acquisitions	Assessment and report			Underway	Several properties under consideration.
Other	Staff	2019-006	Bylaw 1399	Update existing bylaw	2019		Not started	Planned for 2019.
Other	Staff	2019-005	Donation Program	Create program	2019		Not started	Planned for 2019.

**Parks Work Plan
Fall 2018**

PARK PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
Other	Operational		GIS and Mapping	Ongoing mapping			On going	Ongoing
Other			Park wells	well licensing and decommissioning; reconciliation of provincial data			Underway	Park staff to visit possible well sites to confirm if exist and ensure decommissioned if necessary. Provide Province with updated well data.
Other		2018-060	Parks Programming	Programming for all seasons	2018		Underway	Spring & Summer Programming Winter 2018, Fall Programming completed June/July 2018.
Other		2018-052	Policies	Update	2018		Not started	Planned for fall 2018.
Other	Operational		Portfolio Records & Mapping	Acquisition registry, park records, mapping, statistics			On Going	Areas A and B Property Registry Forms and Planning Maps completed and distributed. Area C maps nearing completion; property data collection and naming advanced for Areas E, F and G.
Other	Operational		Project Management	Software	2019		Not started	Planned for 2019.
Other	Operational	2018-022	Standing Contracts	With Purchasing Manager bring on frequently used contractors under a standing contract	2018	2018 Q3	Not started	Planned for fall/ winter
Other	Operational	2017-044	Risk Management	Update Inspection Program	2017	2018 Q4	Underway	Work will continue throughout 2018.
Other		2018-059	School Programming	Working with schools to encourage more interpretive programs	2018		Underway	Program planning Spring 2018, implementation Fall 2018.
Other	Operational	2016-032	SharePoint system	Create and upload old files	2016		Underway	Staff continue to move files from the old files to the new system.
Other		2018-058	Volunteers in Parks	Re-initiate program	2018		Not started	Planned for fall 2018.
Other	Operational	2017-043	Worksafe BC	Update program	2017	2018 Q4	Underway	Work will continue throughout 2018.

TO: B POSAC, EW/PV POSAC, E POSAC, F POSAC, G POSAC, H POSAC, EA A Parks, Recreation and Culture Commission, D69 Recreation Commission, Regional Parks and Trails Select Committee **MEETING:** February 4, 2019

FROM: Wendy Marshall
Manager of Parks Services

SUBJECT: Parks Update Report – October-December 2018

RECOMMENDATION

That the Parks Update Report – October-December 2018 be received as information.

SUMMARY

Parks staff continue to work on projects identified in the Parks Work Plan and the RDN Operational Plan.

BACKGROUND

A summary of key projects undertaken between October and December is provided below. The attached spreadsheet provides details on parks projects.

Planning and Capital Projects – Key Highlights

Mount Benson Parking Facilities RP-S4-1.20

The new parking lot for Mount Benson Regional Park was tendered in late 2018 and awarded at the January 22, 2019 Regional Board Meeting. Construction is scheduled to begin early spring with project completion this summer.

Huxley Park Projects RP-S4-1.2

Sports Court

A contractor has been hired to paint new pickleball lines on the sports court. The work is in the contractor's queue to be completed by summer 2019.

Phase 2 and 3 Construction Drawings

The design consultant continues to work on completing Phase 2 Construction drawings to 100% completion and Phase 3 Construction drawings to 50% completion. Cost estimates will be provided. A Geotechnical Engineer has provided an evaluation of the site, the results of which are being incorporated into the design drawings. A Development Variance Permit for the skatepark has been submitted to the Islands Trust.

Grant Applications

The Regional District of Nanaimo is submitting two grant applications under the Federal/Provincial Investing in Canada Infrastructure Program: the Community, Cultural and Recreation (CCR) Grant to complete Huxley Park Phase 2 which includes a new skatepark, parking lot, park entrance, perimeter fencing; and the Rural and Northern Communities (RNC) Grant, to complete Huxley Park Phase 2 and 3 which includes both the construction of the skatepark, parking lot and fencing, as well as the replacement of the tennis courts and sports court bleachers.

Trail from Horne Lake and Heritage Designation RP-S4-1.12

The Deputy Registrar, Land Titles Victoria, recommends creating a right of way plan for the 1911 Gazetted Horne Lake Road in order to provide certainty as to road location for all parties. The RDN will obtain full boundary identification as opposed to just a centre-line traverse, and surrounding landowner Island Timberlands will gain future subdivision options. The Ministry of Transportation and Infrastructure (MoTI) supports pursuit of this ROW plan. Consultation with Island Timberlands will proceed in early 2019, with a view to achieving a marked ROW and rough footpath on the ground in the fall of 2019.

Staff will meet with Alberni-Clayoquot Regional District staff in early 2019 to develop a framework for engagement with First Nations having interest in heritage designation of the traditional cross-Island passage.

TCT Trail Parking and Trail Realignment RP-S4-1.21

Land access licences were concluded with Island Timberlands and a private landowner for parking area and new access trail to the Haslam Creek Bridge. Phase I parking area and trail signage installation was completed in December 2018. This phase was supported by a \$30,000 grant from BC's Rural Dividends Program. The final phase, including horse trailer parking arrangements worked out in partnership with the mid-Island Back Country Horsemen of BC, will be completed in early 2020.

Anders & Dorrit's Community Park Design RP-S4-1.5

An application for non-farm use to the Agricultural Land Commission was made as the property is in the ALR and will involve the construction of a small (9-stall) gravel parking lot. The Agricultural Advisory Committee attended a site visit and discussed the project. At their December 7th meeting, it was moved that no comment be provided to the ALC on the project. Once a response from the ALC is received, next steps for the project can be refined.

Errington Community Park – Master Planning Process RP-S4-1.6

The Concept Master Plan for Errington Community Park and Order of Magnitude Cost Estimate was completed in December 2018. The year-long process involved community engagement, preliminary concept designs, phasing considerations, and preliminary costing. A staff report regarding proceeding to Detailed Design and Class B Costing is being prepared for Board review.

Moorecroft Washroom Construction and Cabin Removal PR-S4-1.10

A new double-vault toilet building has been completed at Moorecroft Regional Park and the portable toilets have been removed from the Park's entrance area. Ms. Moore's cabin site has been restored with native vegetation and a bench was constructed on site from the chimney and

wooden beams that were salvaged during the cabin's removal. Park staff have been in communication with Nanoose Bay Elementary School about the creation of painted tiles that can be placed on the site to represent Ms. Moore or other aspects of the Park's history or ecology. The tiles and an interpretive sign are planned for 2019.

Little Qualicum River – Crossing Design RP-S4-1.11

Detailed Design and Class B Costing for the Little Qualicum River Regional Park Bridge Replacement project were completed in December 2018. The consultant completed Tender-ready drawings as part of their submission and provided costing for a 2020 construction timeline. A report regarding proceeding to tender in 2020 is being prepared for the Regional Parks and Trails Select Committee in February 2019.

Jack Bagley Park – Court Placement PR-S4-1.16

A site survey was completed in late 2018 and planning for the possible integration of a racquetball court on the property will begin in 2019. The project will involve user-group and stakeholder consultation and preliminary conceptual design to establish what elements to include and where they could be located. It is expected that detailed design of a preferred option would follow.

Village Way Path

Preliminary revised engineered drawings involving simple asphalt curb and drainage system have been accepted by MoTI. The preliminary design is currently being costed by the retained engineering firm. It is anticipated that approval drawings will be submitted to MoTI in early 2019. Preparation of tender ready documents would follow successful project review, confirmation of development budget, and confirmation of permit approval approach by MoTI.

Benson Creek Falls Facilities RP-S2-3.7

Upon final Board approval, a grant application will be submitted under the Federal/Provincial Investing in Canada Infrastructure Program: the Community, Cultural and Recreation (CCR) Grant for the bridge over Benson Creek, the descent to Ammonite Falls and a parking lot on Weigles Road. A grant application will also be submitted to the Island Coastal Economic Trust's Economic Infrastructure and Innovation Program.

Meadowood Way Community Park – Community Centre Development RP-S3-1.7

The Meadowood Community Centre Class B cost estimate for a prefabricated metal building that would serve as a community recreation centre in the Meadowood Community Park on Galvin Road has been received and reviewed. Activities are now focused on achieving a third party operating agreement that needs to be in place prior to advancing the project to tender.

Driftwood Beach Access Development RP-S4-1.1

Trail improvements, including a new aluminum staircase, have been completed at the Driftwood Road Beach access under permit with the MoTI. The stairs now provide a safe descent to the beach. Native vegetation has been planted on site to restore and decommission the steep eroded access that was historically used.

Little Qualicum Hall Renovation RP-S2-3.6

Consultants have completed the drawing package for the Little Qualicum Hall renovation and the building permit process is underway. RDN Parks will continue with Project Management for the demolition of the existing kitchen and washrooms and the construction of the new addition. The start of construction for the new kitchen and washroom addition is slated for 2019.

Dunsmuir Community Park Development RP-S4-1.7

Substantial Completion has been issued for the construction of Dunsmuir Community Park, Phase 1. Further fine grading and drainage works and plantings are planned to be completed in the spring of 2019.

Registry and Mapping

Parks and GIS staff continue to improve park property fields and detail shown in the RDN Web Map, notably accurate and uniform property size data. Draft acquisition maps for Area C and E were produced. Improved coordination between Planning, Parks and GIS is underway regarding 'statutory right of way for park use' acquisitions created through the development process.

Operational – Key Highlights

General Service Calls

The wind storm on December 20, 2018 resulted in the closure of several community and regional parks due to fallen trees. Some areas received extensive damage including parks on Mount Benson (Mount Benson Regional Park and Benson Creek Falls Regional Park), Area F parks (Malcolm and Harris Road Community Parks) and the Arrowsmith CPR Regional Trail (ACPRRT). Staff spent the last days of December visiting sites and assessing the damage. In January, staff will prioritize the work and begin clearing the parks. It is expected that it will take most of January to deal with the storm damage. The ACPRRT, due to the remoteness of the site, will take longer.

Park Inspection and Service Software RP-R2-4.6

In 2018 staff carried out and documented 866 inspection elements, through 296 visits, at 215 park properties including trails and water accesses; and are working towards full compliance with the Park Inspection policy. An enhanced focus on park inspections is proactively identifying potential problems and helping identify conditions for future budget consideration or other improvements in a timely manner.

Parks Programming

Programs in our parks were successful in the last part of the year, with none being cancelled. A Geology Tour ran out of Nanoose Bay, touring through Beachcomber Regional Park and Blueback Community Park in October and was full, with nine registrants. The Amazing Race at Horne Lake Regional Park had four teams and allowed more than 15 people to complete challenges throughout the park. We will continue to improve this program with feedback from participants. The clear weather also allowed us to offer a hike up Mount Horne in October, which ran with five registrants, instructor and volunteer assistant. In November, the Secrets of Salmon Nature Tour guided registrants through parts of Big Qualicum River Regional Trail and the Big Qualicum Fish Hatchery.

Park Use Permits

Area A – Initial work started with the BC Cetacean Sighting Network to develop site specific Whale Trail signage for posting at the Nelson Road Community Boat Launch. The sign will show what can be sighted from there and encourage folks to report all cetacean sightings.

Area B – No permits this quarter.

Area C – No permits this quarter.

Area E – No permits this quarter.

Area F - Permit given to Corcan Meadowood Resident's Association for their 8th annual Halloween Event, taking place partly in Meadowood Community Park and including fireworks.

Area G - Parks Use Permit denied to Mount Arrowsmith Biosphere Region Research Institute for a geocache at Top Bridge Regional Trail. It was deemed that the park and trail is already very well used, and has multiple geocaches already in place.

Area H - No permits this quarter.

FINANCIAL IMPLICATIONS

The projects outlined in this report have funds identified in the 2019 Budget. Electoral Area projects are funded through the associated 2019 Community Parks Budget and in some cases are supplemented by Community Works Funds or grant funding. Regional Parks projects are funded through the 2019 Regional Parks Operational Budget or the Regional Parks Capital Budget.

STRATEGIC PLAN IMPLICATIONS

Projects in this report support the RDN's strategic priorities for Service and Organizational Excellence and Focus on the Environment:

- Delivering efficient, effective and economically viable services that meet the needs of the Region;
- We will fund infrastructure in support of our core services employing an asset management focus;
- As we invest in regional services we look at both costs and benefits — the RDN will be effective and efficient;
- We recognize community mobility and recreational amenities as core services; and
- We will have a strong focus on protecting and enhancing our environment in all decisions.



Wendy Marshall
wmarshall@rdn.bc.ca
January 28, 2019

Reviewed by:

T. Osborne, General Manager, Recreation and Parks

P. Carlyle, Chief Administrative Officer

Attachments

1. Parks Work Plan – Fall 2018

**Parks Work Plan
Fall 2018**

PARKS PROJECTS AND REQUESTS - 2018								
Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
A	16-785	2017-002	Beach Accesses	Driftwood Beach Access consultation and design	2017	2018 Q3	Completed	Board report completed and design to move forward during summer. Design completed.
A	18-149	2018-001	Beach Accesses	Driftwood Beach Access stairs construction	2018	2018 Q2	Completed	Construction underway to be completed this fall.
A	17-153	2018-025	Cedar Plaza	Review Possible Alternatives to allow water	2017	2018 Q2	Underway	Application into NCID waiting for approval.
A	18-224	2018-074	Cedar Plaza	Installation of a communications board			Not started	TBD
A	18-144	2018-026	Glynneath	Trees - removal/replant. Keep park in natural state	2018	2019 Q2	Completed	Harvesting Assessment complete, 2018 planned work complete.
A		2018-024	Kipp Road	Geotechnical options for site remediation	2018	2018 Q2	Underway	Remediation technical plan complete and cost estimate complete. Funding requested to carry out work in 2019
A	17-516	2018-023	Skatepark	Review options for a viewing platform	2018	2019Q3	Not started	Identified in 5-year work plan.
A	18-223	2018-075	Thelma Griffiths	Pilot Park for new signs	2019		Not started	Planned for 2019
B	16-348, 18-008	2017-006	707	New Signs as per plan	2018	2017 Q4	On hold	Delayed until parkland addition brought into the RDN system.
B		2018-027	707	Gate, drainage and road repairs	2018	2018 Q3	Completed	Fire Department Access Improved, road and drainage repairs underway.
B		2018-028	707	New Property - add amenities	2018		Underway	Will start once property added to the RDN system.
B			707	Management Plan Update	2020		Not Started	Planned for 2020.
B			707	New Property development	2023		Not Started	Planned for 2023.
B		2018-031	Cox	New Property - add safety amenities	2018		Not started	Will start once property added to the RDN system.
B			Cox	Master plan	2021		Not started	Planned for 2021.
B			Cox	Property Development	2022		Not started	Planned for 2022.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
B	18-179		Cox	Work with Galtt on trails	2018	2018 Q3	Completed	Staff working with Galtt on trail layout and marking.
B		2018-029	Huxley	Electrical Upgrades Assessment	2018	2018 Q3	Completed	Conceptual Plan complete.
B			Huxley	Grant applications for Skate Park	2019		Underway	Grant applications in process.
B			Huxley	Skatepark Construction	2020		Not Started	Planned for 2020.
B		2018-061	Huxley	Park Opening Event	2018		Completed	Opening event held.
B	18-182	2018-076	Huxley	Pickle ball line painting	2018	2018 Q4	On Hold	On hold due to contractor availability/weather in 2019. To be completed summer 2019.
B	16-346	2016-007	McCollum Road Cash in Lieu	Work with Developer for processing the Cash in Lieu	2016	TBD	Underway	Waiting for payment from developer.
B		2018-033	Pilot Bay and Huxley Benches	Benches	2018	2018 Q3	Underway	Huxley Benches Installed.
B		2018-032	Whalebone	Upgrades to trails	2018	2018 Q2	Completed	Upgrades complete.
B-CWrks	18-180	2018-030	Huxley	Skateboard and park entrance Construction drawings/costing/geotec/survey	2018	2018 Q2	Underway	Consultant hired. To be completed in 2019. Geotech completed.
B-CWrks	Staff	2017-009	Joyce Lockwood Stairs	Rebuild Beach Access stairs	2018	2018 Q3	Suspended	Unfavourable Geotech Report. Replacement will not proceed.
B-CWrks	Board	2015-001	Village Pathway	Design/MOTI approval	2015		Underway	Met w MOTI 18May; identified possible approach. Project engineer revising plans as discussed with MOTI.
B-CWrks		2017-011	Village Way Path	Construction	2019	2020	On hold	On hold until MOTI approval of design is obtained.
C EW/PV	Posac	2018-003	Anders Dorrit	Information sign design and install	2018	2019 Q3	Not started	Planned for 2019.
C EW/PV	Posac	2018-004	Anders Dorrit	Concept Design & Open House	2018	2018 Q3	Completed	Open house held at end of April.

Parks Work Plan Fall 2018

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
C EW/PV			Anders Dorrit	ALC, MOTI approvals	2018	2019-Q2	Underway	Application to ALC submitted. Awaiting response.
C EW/PV			Anders Dorrit	Detailed Design	2019	2019 Q3	Not started	To follow permits and approvals.
C EW/PV			Anders Dorrit	Tender & Construction	2020	2021	Not started	Planned for 2020.
C Ext		2018-035	Extension Miners	Coal artifact display, trail to river design	2018	2018 Q4	Not started	Now planned for 2019.
C Ext			Extension Miners	Install Trail to river	2019		Not started	Planned for 2019.
C Ext		2018-034	Trails Initiative	Horse Access on Crown	2018	2018	Underway	Coordinating with Backcountry Horsemen of BC rep.
E	18-084	2018-055	Bonnington Drive/Coventry Place	High level report on cost and process for stairs and trail construction	2018	2018 Q3	Completed	Report presented at fall POSAC. In work plan for 2021.
E	18-085	2018-037	Brickyard	Parking, Toilet, Garbage - Concept Layout & Neighbour Consultation	2018	2018 Q2	Suspended	Suspended as per board direction.
E	18-085	2018-038	Brickyard	Toilet, Garbage Install	2018	2018 Q3	Suspended	Suspended as per board direction.
E	16-197	2016-009	Claudet	Utilities lot transfer	2016	TBD	Underway	Staff reviewing approach to dispose of parkland.
E			Claudet	Master Plan	2023		Not started	Added to five year plan as per POSAC request.
E		2018-039	Es-hw Sme~nts	Interpretive signs -design and install	2018	2018	Completed	Completed.
E	18-087	2018-083	Jack Bagley	Court placement and design	2019		Underway	Survey completed in 2018 and planning in 2019.
E	17-155, 17-409	2018-036	Nanoose Road Community Park	Lease renewal, public consultation, Design new use once received	2018	2020	Not started	Option to purchase to be considered.
E	17-517, 18-086	2018-074	Stone Lake Drive	Natural Playground Site survey, Public Input & Preliminary Design	2018	2019Q2	Underway	Survey and Focus Group completed. Design and public consultation in 2019.
			Water Accesses	Inventory by POSAC Sub-Committee	2017	2018 Q4	Completed	Inventory completed. Project report to Feb 2019 POSAC.
E			Water Accesses	Priority from Study (sign some existing access points)	2019	2019 Q4	Not started	Planned for 2019.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
E			Water Accesses	Priority from Study (survey, design, install) (Ted's Road)	2021		Not started	Planned for 2021.
F	17-310	2018-009	ACT Trails	Surface David Lundine trail	2018	2018 Q2	Completed	Trail construction underway. Will be completed by the end of October.
F		2018-040	ACT Trails	Palmer Road East Trail Construction	2018	2018 Q4	Underway	Topo survey underway.
F			ACT Trails	Bellevue Trail 1 - Grafton/Bellevue	2019		Not started	Planned for 2019.
F			ACT Trails	Bellevue Trail 2 - Koperick link	2019		Not started	Planned for 2019.
F	17-312	2018-006	Errington	Park Master Plan to include a bike skills park and a playground	2017	2019 Q3	Completed	Final Concept Master Plan underway. Survey, Geotech rev, and costing complete.
F			Errington	Phase I detail design	2019		Not started	Planned for 2019.
F			Errington	Phase II Detail Design	2021		Not started	Planned for 2021.
F			Errington	Phase II Construction	2022		Not started	Planned for 2022.
F	Staff	2018-007	Errington	Operator Agreement	2018	2018 Q4	Underway	Due to water/septic infrastructure staff investigating options including Crown grant.
F	18-183	2018-077	Malcolm	Pilot Park for new signs	2019		Not started	Planned for 2019.
F-CWrks	18-196	2018-041	Meadowood	Community Hall Construction	2018		Underway	Schematic design and Class B estimate complete. Hall operator agreement drafting underway to be followed by a Board report.
F-CWrks	Posac	2019-001	Errington	Phase I construction	2020		Not started	Planned for 2020.
G		2018-056	Blue Water	Clearing and restoration.	2017	2020 Q3	Completed	Vegetation management and planting complete for 2018.
G		2017-015	Boulton	Replace playground borders	2017	2018	Completed	Work carried out by Ops Staff
G			Dashwood	Playground Updates	2019		Not started	Planned for 2019.
G		2018-057	Lee Rd	Tree removal French Creek	2018	2018 Q1	Completed	Trees removed from the creek.
G	18-185	2018-078	Maple Lane	Develop plan and add to five year plan	2018	2018 Q3	Completed	Added to five year plan.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
G	18-184	2018-079	Maple Lane	Pilot Park for new signs	2018	2018 Q3	Underway	Signs under development
G	Posac	2018-010	River's Edge	Playground concept design	2019		Not started	Planned for 2019.
G			River's Edge	Playground detail construction drawings	2020		Not started	Planned for 2020.
G			River's Edge	Playground Install	2021		Not started	Planned for 2021.
G-CWrks	Posac	2019-002	French Creek Paths/Trails	TBD	2020		Not started	Planned for 2020.
G-CWrks	17-158, 17-405, 17-600	2018-011	Little Qualicum Hall	Engineering study and community consultation to determine direction	2018	2018 Q3	Completed	Engineering study complete and consultation underway with community. Report complete and direction determined.
G-Wrks	18-295	2018-084	Little Qualicum Hall	Undertake safety and accessibility upgrades	2018	2019 Q3	Underway	Concept plan complete and approved. Plans are with engineering consultant for permit drawings.
H		2018-042	Beach Accesses	Signage and Improvements	2018	2018 Q3	Underway	Beach Access priorities determined at June 11 POSAC. Signs will be installed in fall/winter.
H	17-020	2017-016	Dunsmuir	Detailed design	2018	2018 Q2	Completed	Phase 1 tender drawings completed and project tendered.
H			Dunsmuir	Phase II Planning	2019		Not started	Planned for 2019.
H			Dunsmuir	Phase II Construction	2020		Not started	Planned for 2020.
H		2016-013	Lions Park	Operators Agreement	2016	2018 Q3	Underway	Revised licence sent to Lions and follow up meeting taken place. Agreement from Lions expected by February.
H	Posac	2019-003	Oakdowne	Licence on other crown parcels surrounding the park	2019		Not started	Planned for 2019.
H			Sunny Beach	Improvements	2019		Not started	Planned for 2019.
H	Posac	2018-013	Wildwood	Kiosk development and install - split with Regional	2018	2018	Underway	Artwork for sign is under review.
H	18-143	2018-080	Wildwood	Pilot Park for new signs	2018	2018 Q4	Underway	Planned for fall/winter in conjunction with kiosk design.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
H-CWrks	Other	2020-001	Area H Roadside Trails	Work with MOTI on any development of trails along the road.	2020		Not started	Planned for 2020.
H-CWrks	18-148, 18-147	2018-012	Dunsmuir	Phase I construction	2018	2018 Q3	Underway	Substantial completion issued. Final completion spring 2019.
Other - Comm	Other	2018-014	Bike Network Plan	Develop plans	2019		Not started	Planned for 2019.
Other - Comm	Operational		Development	Subdivision/parkland - review, comments and POSAC input			On Going	Ongoing as required through planning.
Other - Comm	16-616, 18-061	2016-018	Park Signage	Redesign Park regulation and info signs	2016	2018 Q3	Completed	Board and committees have reviewed and pilot parks selected.
Other - Comm	18-357,		Park Signage	Install new signs in pilot parks in each EA			Underway	First sign designs underway
REG	Staff	2017-019	Ammonite Falls Trail	Bridge Replace	2017	2019 Q3	Underway	Environmental Assessment complete. Design estimates underway, construction in 2019 pending funding.
REG	Staff	2018-020	Arboretum	Kiosk upgrade -design new sign with volunteers and install	2018	2018 Q4	Underway	Design of signage has started.
REG	Mngmnt Plan	2018-045	Beachcomber	Main Path Upgrades (stairs/handrails)	2018	2018 Q4	Completed	Handrail installed, trail surfacing and box stairs complete.
REG	18-072	2016-028	Beachcomber	Management Plan	2016	2017	Completed	Board approved plan.
REG	16-669	2017-048	Benson Creek Falls	Parking feasibility study and design - Weigles entrance (incl. env. study)	2017	2018 Q1	Completed	Concept planning complete. Approvals, detailed design to follow.
REG	16-668	2017-049	Benson Creek Falls	Parking - monitor and communicate with residents	2017		Ongoing	Ongoing.
REG			Benson Creek Falls	Weigles entrance parking Lot Construction	2019		Not started	Will occur with stair and bridge construction.
REG			Benson Creek Falls	Grant applications for bridge, stairs and parking	2019		Underway	Grant applications in process.
REG	Mgmt. Plan	2018-019	Benson Creek Falls	Kiosk	2020	2020	Not started	Planned for 2020.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG			Benson Regional Trail	Signage	2019	2020	Not started	Planned for 2019.
REG		2018-043	BigQ/ACRD Regional Trail	Detailed planning	2018		Not started	Will proceed once preliminary work is completed.
REG		2020-003	BigQ-ACRD Regional Trail	Construction	2019	2020	Not started	Would take place after detailed design.
REG		2018-048	Coats Marsh	Building and Bat housing	2018		Underway	Bat house installed. House removal by end of Jan.
REG	Staff	2018-018	Descanso	Tree removal	2018	2018 Q2	Completed	Coastal Wildfire carried out Phase 1, Phase 2 underway by contracted arborist to be completed in fall.
REG			Descanso	Gate house replacement	2020		Not started	Planned for 2020.
REG		2018-046	Englishman River	Geo technical study for the bank above the road	2018	2018 Q3	Completed	Geotech Consultant to site, assessment underway, to be completed by year-end
REG	Other		Fairwinds	Development - PDA		TBD	Underway	Staff have met with Planning Staff to review development proposal and PDA amendments.
REG	16-767	2017-024	First Nations Passage via Horne Lake	Heritage designation	2017	2020	Underway	Parks to arrange mtg w ACRD staff in New Year n discuss approach.
REG			Horne Lake	Picnic shelter design/tender	2021		Not started	Planned for 2021.
REG		2018-044	Horne Lake and Descanso	Registration Software	2018	2018 Q2	Underway	Registration software implementation underway. Supplier delays. To be completed in 2019.
REG		2017-021	La Selva Trail	Construction	2017		Delayed	Delayed due to land issue at site.
REG			Lighthouse	Nile Creek Bridge	2021		Not started	Planned for 2021.
REG			Lighthouse	Fletcher Creek	2023		Not started	Planned for 2023.
REG	16-480	2016-026	Moorecroft	Site Planning with First Nations (as per workshop priorities)	2016	2017 Q4	Ongoing	Consultation underway when required.
REG	Mgmt. Plan	2018-021	Moorecroft	Picnic shelter design/tender	2020		Not started	Planned for 2020.
REG			Moorecroft	Parking Lot Design & Improvements	2021		Not started	Planned for 2021.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG			Moorecroft	Meadow Road Upgrade	2019	2019 Q2	Not started	Planned for 2019.
REG			Moorecroft	Reroute Water Line	2019	2019 Q2	Not started	Planned for 2019.
REG			Moorecroft	Entry Area Improvements	2020		Not started	Planned for 2020.
REG		2018-049	Moorecroft	Miss Moore's Cabin Removal	2018	2018 Q3	Completed	Cabin removed and site restoration complete. NBES to paint tiles for site in 2019.
REG			Moorecroft	Interpretive Signage (design & install)	2021		Not started	Planned for 2021.
REG			Moorecroft	Kiosk	2020		Not started	Planned for 2020.
REG	Mgmt. Plan	2017-041	Moorecroft	Two vault Toilets & storage (design, locate, install)	2017	2018 Q3	Completed	Archeologist review of site complete, vaults installed, building complete.
REG	16-666, 17-510	2018-054	Mount Benson	Parking design and construction	2018	2019Q3	Underway	House removal complete. MOTI permit received. Tender complete, report to Board for approval in Jan. 2019
REG			Mount Benson	Trail Improvements - Condition and Evaluation of High Use Routes	2019		Not started	Planned of 2019.
REG			Olympic Torch Trail and Thames Crk	Planning	2020		Not started	Some reviews done for seniors housing project.
REG			PQ Links	Barclay Bridge - asphalt extension	2019		Not started	Planned for 2019.
REG	Staff	2017-020	TCT	Timberlands Road trail head development	2016	2018 Q4	Completed	Draft license for landowner and Island Timberlands awaiting Board approval. Detailed planning of parking lot completed. Rural Dividend Program updated.
REG		2018-080	TCT signage review	GPSing of signage in prep for updating given new TCT signs.	2018		Underway	Staff GPSing signs.
REG			TCT signage update	Apply for free signage if available; otherwise purchase. Update all.	2019		Not started	Planned for 2019.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG	Other	2019-004	Top Bridge	Reroute trail and parking lot in City of Parksville	2021		Not started	Timing depends on the City of Parksville.
REG			Top Bridge	Connection to Rail Trail - Design	2019		Not started	Planned for 2019.
REG			Top Bridge	Connection Construction	2020		Not started	Planned for 2020.
REG	Posac	2018-017	Wildwood/LHR T	Kiosk development and installation - split with H	2018	2018	Underway	Artwork for sign is under review.
REG	18-176	2017-070	Witchcraft Lake Reg. Trail	Volunteer Trail Building Agreement	2017	2018 Q1	Completed	Agreement approved and executed by Board.
REG	18-176	2018-047	Witchcraft Lake Reg. Trail	Volunteer Trail Building	2018	2018 Q3	Underway	Agreement complete, logistical planning and material assembly underway, fire ban delayed start.
REG	18-177	2018-081	Witchcraft Lake	Costing and location for moving boardwalk	2018	2018 Q4	Underway	Working with City of Nanaimo on location and costs.
REG CAP		2018-050	Benson Creek Falls	Detailed design and costing stairs and bridge	2018	2019Q3	Not started	Planned for 2019.
REG CAP			Benson Creek Falls	Bridge Construction	2019	2019/2020	Not started	Planned for 2019.
REG CAP			Benson Creek Falls	Stairs at falls Construction	2019	2019/2020	Not started	Planned for 2019.
REG CAP	16-670	2017-031	Benson Creek Falls	Concept Plans and consultation Bridge and Stairs	2017	2018 Q4	Completed	Concept plans and public consultation completed. Report went to October RPTSC, forwarded to Board.
REG CAP		2017-043	BigQ/ACRD Regional Trail	Preliminary planning including agreement with strata	2017		Underway	Brief provided to Strata received with no comment. MoTI advises RDN to negotiate 1911 Horne Lake Rd plan revisions with Island Timberlands.
REG CAP	17-227	2017-073	E&N -- Coombs to Parksville Rail Trail	Horse Parking - Wood lot	2017		On Hold	Awaiting Springhill Rd development.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG CAP		2018-062	E&N -- Coombs to Parksville Rail Trail	Trail Repair - Clean up, Design and rebuild		2018 Q3	Completed	Final design being detailed with input from environmental monitor; subject to approval by ICF and SVI. Building contractor lined up. Work to be completed by end of October.
REG CAP		2017-022	E&N -- Coombs to Parksville Rail Trail	Finish Construction Details	2018		Completed	First review of as-built plans completed. Final plans await completion of flooding repairs.
REG CAP	17-386	2017-068	Little Qualicum	Bridge Design	2018	2019	Completed	Survey and geotech review underway. Detail Design and Costing to be complete by November.
REG CAP			Little Qualicum	Bridge Construction	2020	2020 Q4	Not started	Planned for 2020.
REG CAP	Board	2014-001	Morden Colliery	Lease upgrade	2014		Underway	Staff working with Province on lease upgrade request.
REG CAP	16-124,14-755,14-754	2017-047	Morden Colliery	Bridge and trail design and tender	2017		Underway	ALC provided approval early July, with development conditions and July 2021 deadline for completion. RFP development underway.
REG CAP			Morden Colliery	Construction	2019	2020	Not started	Will commence after detailed designs are completed.
REG - Other	Staff	2018-053	Brochure	Design New Brochure/Print	2017	2018 Q3	Completed	Guides available in hardcopy or online.
REG - Other	Operational		Operator agreements	Monitor			Ongoing	Ongoing
REG - Other	Other	2018-016	Park Zoning	With Planning Department	2019		Not started	Planned for 2019.
REG - Other	Operational		Partnerships	Meetings and on-going communication with partners			Ongoing	Ongoing
REG - Other	Operational	2017-042	RPT Plan	Develop RFP. Work with consultant on plan development	2018	2019 Q4	Not started	RFP to be prepared during fall for Board report in the spring of 2019.

Parks Work Plan Fall 2018

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
REG - Other	16-654, 16-678, 17-019, 18-030	2016-020	Salish Sea Marine Trail	Agreement with BC Marine Trail Association & signage	2016	2018 Q3	Completed	Signage received from BCMTNA and installed at five approved locations. DBRP pamphlet updated to reflect being on Salish Sea Marine Trail.
REG - Other		2017-072	Tourism VI Trails Strategy	Phase 3 Exceptional Hiking Experiences Network	2017		Underway	Met w TVI n VISTA to encourage TVI focus on this multi-region trail.
Other	Operational	2018-051	2019 Budget and Work plan	Create and forward for approvals	2018	2019 Q1	Completed	Budgeting for 2019 underway.
Other	18-217	2018-082	Wicklow West land exchange	Work with developer on land exchange	2018	2019 Q2	Underway	Subdivision applications submitted. Working on process for disposition of parkland.
Other			Acquisitions	Assessment and report			Underway	Several properties under consideration.
Other	Staff	2019-006	Bylaw 1399	Update existing bylaw	2019		Underway	Contractor hired to take on project; parks n bylaw briefing mtgs held Nov-Dec.
Other	Staff	2019-005	Donation Program	Create program	2019		Not started	Planned for 2019.
Other	Operational		GIS and Mapping	Ongoing mapping			On going	Ongoing
Other			Park wells	well licensing and decommissioning; reconciliation of provincial data			Underway	Park staff to visit possible well sites to confirm if exist and ensure decommissioned if necessary. Provide Province with updated well data.
Other		2018-060	Parks Programming	Programming for all seasons	2018		Completed	Spring & Summer Programming Winter 2018, Fall Programming completed June/July 2018.
Other		2018-052	Policies	Update	2018		Not started	Planned for fall 2018.
Other	Operational		Portfolio Records & Mapping	Acquisition registry, park records, mapping, statistics			On Going	Areas A and B Property Registry Forms and Planning Maps completed and distributed. Area C maps nearing completion; property data collection and naming advanced for Areas E, F and G.

**Parks Work Plan
Fall 2018**

Service Area	Origin	Project #	Park	Description	Start Date	Completion	Progress	Comment
Other	Operational		Project Management	Software	2019		Not started	Planned for 2019.
Other	Operational	2018-022	Standing Contracts	With Purchasing Manager bring on frequently used contractors under a standing contract	2018	2018 Q3	Not started	Planned for fall/ winter
Other	Operational	2017-044	Risk Management	Update Inspection Program	2017	2018 Q4	Underway	Work will continue throughout 2018.
Other		2018-059	School Programming	Working with schools to encourage more interpretive programs	2018		Completed	Program planning Spring 2018, implementation Fall 2018.
Other	Operational	2016-032	SharePoint system	Create and upload old files	2016		Underway	Staff continue to move files from the old files to the new system.
Other		2018-058	Volunteers in Parks	Re-initiate program	2018		Not started	Planned for spring 2019.
Other	Operational	2017-043	Worksafe BC	Update program	2017	2018 Q4	Underway	Work will continue throughout 2018.

TO: Regional Parks and Trails Select Committee **MEETING:** February 5, 2019

FROM: Renée Lussier
Parks Planner

SUBJECT: Little Qualicum River Regional Park Bridge Replacement Detailed Design and Class B Costing

RECOMMENDATIONS

1. That the Little Qualicum River Regional Park Bridge Replacement proceed to Tender in February 2020 for construction in 2020.
2. That the 2020-2024 Financial Plan include \$839,910 in the 2020 year for completion of the LQRRP Bridge Replacement.

SUMMARY

The Regional District acquired the Little Qualicum River property in 1999 as parkland dedication from the Qualicum River Estates subdivision process. The acquisition also included a haul bridge across the river. Ozero Sand & Gravel, through affiliation with Wicklow West Holdings, controlled and maintained the bridge and the gravel road from Corcan Road to Melrose Place, by way of Easement over the bridge. After the haul bridge was damaged, it was returned to the Regional District, and the Easement was eliminated.

The bridge at Little Qualicum River Regional Park (LQRRP) was removed due to safety concerns in 2017. A need for a restored crossing to address recreational access, environmental concerns, and emergency access prompted the Board in July 2017 to include the design and construction of a new bridge in the Five-Year Regional Parks Capital Plan. The proposed Detailed Design and Class B Costing for a new crossing was completed by Herold Engineering in the winter of 2018. The cost to construct a dual steel girder and cast-in-place concrete deck bridge is \$839,910 (projected for a 2020 construction start). The 2019-2023 preliminary Five-Year Financial Plan has \$750,000 identified in 2020 for bridge construction in the Regional Parks Capital Budget. This amount will need to be increased to \$839,910 for the 2020 year when the 2020-2024 Financial Plan is produced.

BACKGROUND

The LQRRP is a 44 hectare (108.5 acre) riparian corridor property located in Electoral Area F. It protects 2.7 km of valuable riparian margin along the middle reaches of the river.

The Regional District acquired the Little Qualicum River property in 1999 as parkland dedication from the Qualicum River Estates subdivision process. The Park is bordered by private residential lands, private development lands (Wicklow West Holdings), the Little Qualicum Falls Provincial Park to the south, and Island Timberlands property to the northeast. Meadowood

Community Park also adjoins the LQRRP along Galvin Place (Attachment 1). The acquisition also included a haul bridge across the river.

On the east side of the river, the gravel road passes through Island Timberlands land and Crown land before abutting Melrose Place. On the west side, the road is steep and winding eventually ending at Corcan Road.

The old bridge was used, controlled, and maintained for gravel extraction operations, by way of Easement over the bridge. Ozero Sand & Gravel, through affiliation with Wicklow West Holdings, controlled and maintained the bridge and the gravel road from Corcan Road to Melrose Place. The old bridge had become damaged, was returned to the Regional District, and the Easement was eliminated. There are two gates located at the end of Corcan Road and near the old gravel pit on the east side of the river that remain closed and locked. The gates do not prevent ATV traffic from entering the park along the gravel road.

The LQRRP bridge was reviewed by a structural consultant in December 2014 and options to repair it, upgrade it, or replace it were explored and costed. Based on the consultant report, the Board approved upgrading the bridge for pedestrian, cyclist, service vehicle and emergency use (but not seismic upgrades). A Tender was issued to upgrade the bridge; however, as the bridge had sustained more damage than anticipated, the bids received were in excess of what the full replacement cost would be. The Tender was cancelled and in July 2017 the Board issued a resolution to remove the bridge due to safety concerns. With the need to have a restored crossing, the Board issued the following resolution in July 2017 (17-386):

It was moved and seconded that the design and construction of a new bridge for the Little Qualicum River Regional Park be considered for inclusion in the 5-year Regional Parks Capital Plan.

A restored crossing would address recreational access, environmental concerns, and emergency access around the Little Qualicum River.

Recreational Access

Without a crossing over the river, pedestrians, cyclists, and equestrians are not able to access both sides of LQRRP, therefore, limiting the recreational value of this park. There are popular swimming spots downstream of the proposed bridge location. If these swimming spots increase in popularity and development of a day use area becomes a high priority in the community, it would be beneficial to have the bridge in place for pedestrian access and service vehicle use.

There is a high use of ATVs in this area. While ATVs are not permitted in RDN Parks and Trails, the LQRRP management plan speaks to future Board consideration for a unique allowance for ATV use of this bridge for access to Crown Lands to the south where licenced ATV use is permitted. One issue with this is the indirect granting of ATV access onto the adjacent private lands. Further discussions with the adjacent land owners will need to be undertaken prior to ATV access being granted. In addition, the RDN's Park Use Regulation Bylaw is currently being reviewed in 2019 as part of a larger update that will include consideration of licenced ATV use on LQRRP's primary service road to access adjacent Crown Lands.

Environmental Concerns

The Little Qualicum River includes salmon spawning channels, sensitive riparian habitat, and steep slopes. Without a bridge in place, ATVs are crossing through the river disturbing the spawning channels and eroding the river's banks. By providing a safe crossing with a new bridge, the environmental impact on the river will be reduced and the adjacent sensitive habitats will be better protected.

Emergency Access

The Corcan/Meadowood neighbourhood was identified in the 2018 Operational plan as a priority area requiring evacuation route planning. Efficient and safe evacuation routes are needed for the movement of people, livestock, animals and personal property from an area of imminent or actual threat to an area of safety. RDN Emergency Services has consulted with the first responder agencies in the area (BC Wildfire, BC Ambulance, Arrowsmith SAR, and Dashwood Fire Department) and has confirmed that the proposed bridge will meet the vehicle specification requirements for response to the area.

The construction of a bridge over the Little Qualicum River could provide an alternative access route for emergency response. However, the road is in poor condition and would require significant upgrades and yearly maintenance prior to it becoming a viable evacuation route.

Bridge Design & General Construction Timeline

An RFP was issued by RDN Parks on June 11, 2018 and closed July 13, 2018. The intent of the process was to engage a consulting team that would lead the entire project from start to finish, proceeding only with Phase 1 of the work in 2018. Phases of the project include:

- Phase 1 – Preliminary and Detailed Design. *Includes Class B Costing.*
- Phase 2 – Tender Documents and Tender Administration.
- Phase 3 - Construction Services. *Includes Contract Administration and project close-out documentation.*

In the fall of 2018, Herold Engineering was awarded the project and began work on Phase 1.

Attachment 2 is the Detailed Design drawing package for the LQRRP Bridge. It is a 36.2 m, dual steel girder and cast-in-place concrete deck bridge design. It meets the current CSA Bridge Code and BC Ministry of Transportation and Infrastructure standards for low volume roads. It is a clear-span structure. The bridge is intended to primarily accommodate maintenance/service vehicles and emergency vehicles up to 30,000 kg but will also need to accommodate standard vehicles, pedestrians, cyclists, equestrians, and ATVs.

The general construction timeline for a bridge of this size is six to eight months. A common schedule would be April-June for document submittals and reviews as well as material sourcing and fabrication of the components. On site work is normally done from July to October, with in-stream work generally limited to mid-July to mid-September. Tenders are typically issued between November and February to meet this construction timeline.

ALTERNATIVES

1. That the Little Qualicum River Regional Park Bridge Replacement proceed to Tender in February 2020 for construction in 2020.
2. That the 2020-2024 Financial Plan include \$839,910 in the 2020 year for completion of the LQRRP Bridge Replacement.
3. That alternative direction be provided.

FINANCIAL IMPLICATIONS

The amount of the Class B Costing is \$839,910 for construction. Attachment 3 provides further cost detail. This cost includes provisions for Engineering and Project Management services, General Contractor fees, licensing and permitting costs, as well as a construction contingency of 20%. An additional cost for road repair to accommodate construction vehicles has been included in the bridge construction budget. The Class B Costing assumes a construction start in 2020. Should the construction start in a subsequent year, 8% per year should be added to the cost projection.

The 2019-2023 preliminary Five-Year Financial Plan has \$750,000 identified in 2020 for bridge construction in the Regional Parks Capital Budget. This amount will need to be increased to \$839,910 for 2020 year when the 2020-2024 Financial Plan is produced.

Maintenance and life-cycle management have been considered and bridge replacement has been estimated at 50 years, with no major repairs required for 30 years (See Table 1). These costs will be included in the 2020 Regional Parks Operations Budget.

LQRRP Bridge Operations Budget – Table 1

<i>Item</i>	<i>Frequency</i>	<i>Annual Cost</i>
Periodic inspections by staff	monthly	30 hrs
Periodic inspections by an engineer	every 3 yrs	\$600.00
Maintenance budget @ 1% construction cost • Bank erosion repair • Guardrail maintenance • Signage updates, replacement • Pothole maintenance	annually	\$5,287.40
Bridge replacement budget, 50 years	annually	\$10,574.80
TOTAL		\$16,462.20 plus 30 hrs staff time

Consultant fees were budgeted at \$125,000 for the bridge design. Of that total, \$73,296 was used to complete Phase 1 leaving \$51,704 to complete tender documents and to administer the tender.

STRATEGIC PLAN IMPLICATIONS

The proposed bridge for LQRRP addresses the recreational, environmental, and emergency services needs of the Regional District in the following ways:

Focus On Service And Organizational Excellence - We Recognize Community Mobility And Recreational Amenities As Core Services

Focus On The Environment- We Will Have A Strong Focus On Protecting And Enhancing Our Environment In All Decisions

Focus On The Environment- We Will Prepare For And Mitigate The Impact Of Environmental Events

Focus On Service And Organizational Excellence - We View Our Emergency Services As Core Elements Of Community Safety



Renée Lussier
rlussier@rdn.bc.ca
January 15, 2019

Reviewed by:

- W. Marshall, Manager, Parks Services
- J. Bradburne, Director of Finance
- T. Osborne, General Manager, Recreation and Parks
- P. Carlyle, Chief Administrative Officer

Attachments

1. LQRRP Context Map
2. LQRRP Bridge Replacement Detailed Design drawings
3. Class B Costing



REGIONAL
DISTRICT
OF NANAIMO

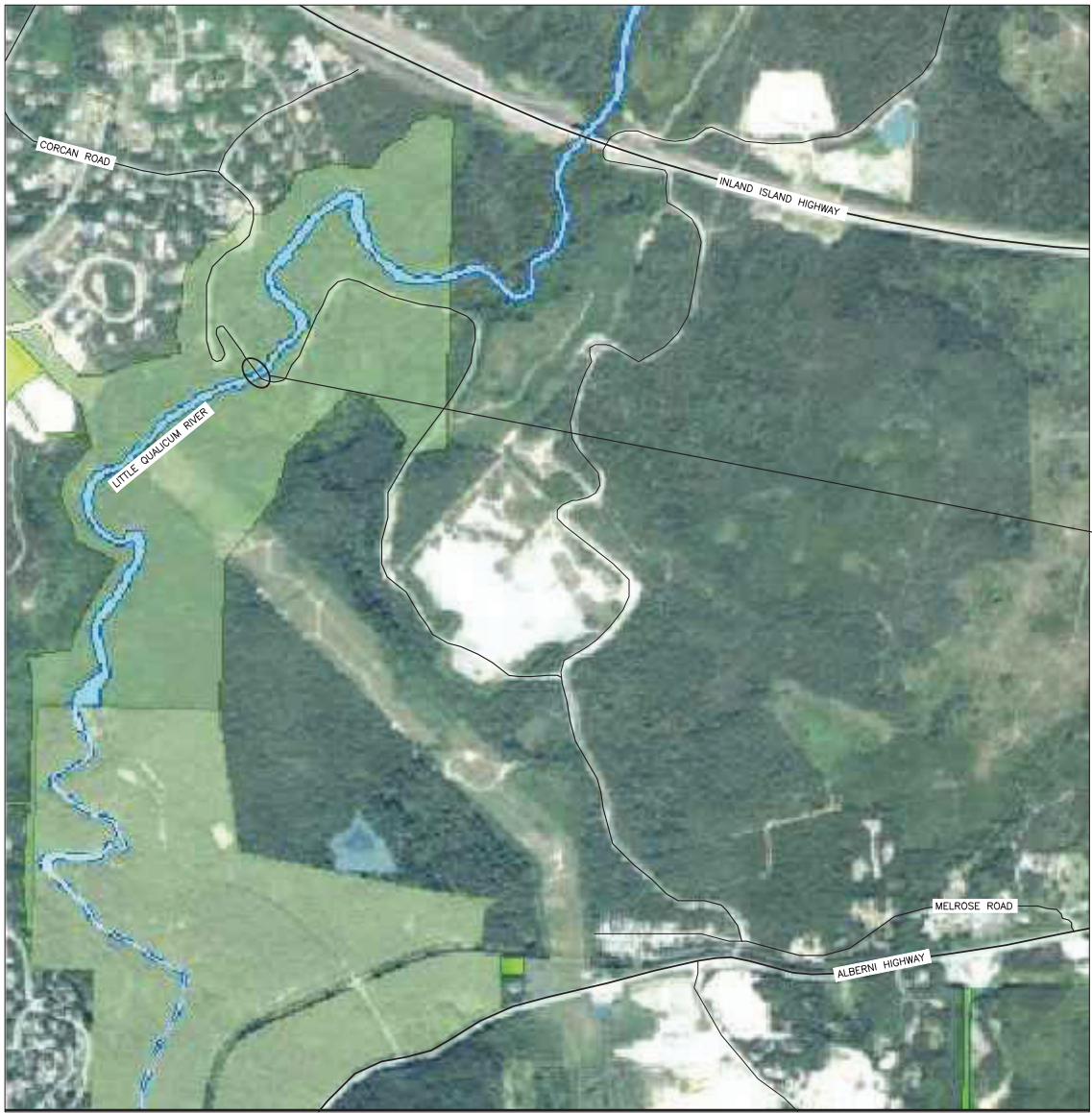
LQRRP Bridge Replacement CONTEXT MAP

not to scale

January 10, 2019



LITTLE QUALICUM RIVER PEDESTRIAN BRIDGE



DRAWING LIST

<u>DRAWING NUMBER</u>	<u>DESCRIPTION</u>
0837-053 S00	COVER SHEET, KEY PLAN AND DRAWING LIST
0837-053 S01	GENERAL NOTES – SHEET 1
0837-053 S02	GENERAL NOTES – SHEET 2
0837-053 S03	EXISTING SITE PLAN
0837-053 S04	GENERAL ARRANGEMENT
0837-053 S05	ENVIRONMENTAL MITIGATION PLAN
0837-053 S06	CAST IN PLACE ABUTMENTS
0837-053 S07	STEEL GIRDERS – SHEET 1
0837-053 S08	STEEL GIRDERS – SHEET 1
0837-053 S09	CONCRETE BRIDGE DECK AND REMOVABLE BOLLARD
0837-053 S10	GUARDRAILS



3701 Shenton Rd, Nanaimo, BC V9T 2H1
Tel: 250-751-8558 Fax: 250-751-8559
Email: mail@heroldengineering.com

HEROLD PROJECT NO.
0837-053

GENERAL

1. DESIGN HAS BEEN COMPLETED IN ACCORDANCE WITH THE 2014 EDITION OF THE CANADIAN HIGHWAY BRIDGE DESIGN CODE CSA S6, EXCEPT AS NOTED BELOW:
- ☐ TEMPORARY OR SHORT TERM INSTALLATION LESS THAN 20 YEARS

☐ PERMANENT BUT WITH A TARGET SERVICE LIFE OF 30-50 YEARS

☒ PEDESTRIAN BRIDGE WITH INFREQUENT VEHICLE ACCESS FOR MAINTENANCE/EMERGENCY

☐ PEDESTRIAN BRIDGE WITH NO VEHICLE ACCESS

☐ LOW VOLUME, LOW VELOCITY ROAD (MODIFIED OR NO VEHICLE GUARDS)

☐ SERVICE OR INDUSTRIAL ROAD, NOT GENERALLY ACCESSED BY THE PUBLIC

2. CONSTRUCTION SHALL BE IN ACCORDANCE WITH CSA S6, INCLUDING ADDENDA, REFERENCED CODES AND ALL FEDERAL, PROVINCIAL AND MUNICIPAL REGULATIONS AND BY-LAWS.

3. CONSTRUCTION SHALL MEET SECTIONS OF THE BC MINISTRY OF TRANSPORTATION STANDARD SPECIFICATIONS INDICATED HERE:

- ☒ SECTION 165 PROTECTION OF THE ENVIRONMENT

☒ SECTION 202 GRANULAR SURFACING, BASE AND SUBBASES

☒ SECTION 205 RIP RAP

☒ SECTION 211 PORTLAND CEMENT CONCRETE

☒ SECTION 216 COATING OF STEELWORK

☒ SECTION 412 CONCRETE REINFORCEMENT

☒ SECTION 413 BRIDGE DECKS & CONCRETE OVERLAYS

☒ SECTION 415 PRECAST & PRE-STRESSED CONCRETE
- ☒ SECTION 308 COATING SYSTEMS

☒ SECTION 418 DECK SEALANTS

☐ SECTION 419 DECK WATERPROOFING

☒ SECTION 421 STRUCTURAL STEELWORK

☒ SECTION 422 MISC STEELWORK

REFERENCE IN THE STANDARD SPECIFICATIONS TO "THE MINISTRY" AND "THE MINISTRY REPRESENTATIVE" SHALL BE TAKEN TO MEAN "THE OWNER" AND "THE OWNER'S REPRESENTATIVE", RESPECTIVELY.

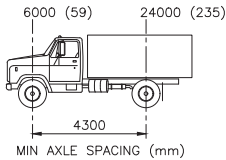
5. REFERENCED CODES AND STANDARDS ON THESE NOTES AND DRAWINGS SHALL BE AS REFERENCED IN CURRENT EDITION OF CSA S6 AND THE STANDARD SPECIFICATIONS

6. DESIGN LOADS:

PROJECT LOCATION: PARKSVILLE/QUALICUM BEACH								
SNOW LOADS		PEDESTRIAN LOAD		SEISMIC				
Ss	2.4 kPa	LL	4.0 kPa	BRIDGE CLASS		OTHER		
Sr	0.4 kPa			SITE CLASS		SEE GEOTECH		
WIND LOADS		WEARING SURFACE		R VALUE		N/A (SINGLE SPAN)		
q10	0.41kPa	MATERIAL	ASPHALT	EQ		0.8 x DL (AT ABUT'S)		
q50	0.53kPa	THK	N/A	Sa (0.2)	Sa (0.5)	Sa (1.0)	Sa (2.0)	PGA
		DL	N/A	0.86	0.61	0.32	0.17	0.42

* DESIGN VEHICLE:

30000 kg GVW ON 2 AXLES:
MAX AXLE LOADING, kg (kN)



* NOTE: LOAD RATING IS FOR FULLY COMPOSITE DECK. FATIGUE DESIGN HAS NOT BEEN INCORPORATED INTO THIS STRUCTURE. INFREQUENT CYCLES OF FULL LIVE LOAD ARE EXPECTED.

7. THESE DRAWINGS INCLUDING DIMENSIONS SHALL BE READ IN CONJUNCTION WITH ALL OTHER PROJECT DRAWINGS AND SPECIFICATIONS. CONTRACTOR SHALL REPORT ANY DISCREPANCIES TO THE STRUCTURAL ENGINEER FOR CLARIFICATION PRIOR TO COMMENCING CONSTRUCTION. CONTRACTOR SHALL BE FAMILIAR WITH ALL PROJECT DRAWINGS INCLUDING THOSE OF OTHER DISCIPLINES AND SHALL MAKE ALLOWANCES FOR ALL ITEMS SHOWN ON OTHER DRAWINGS THAT AFFECT THIS CONTRACTOR'S WORK.
8. THESE DRAWINGS SHOW THE COMPLETED STRUCTURE ONLY. PROVIDE TEMPORARY BRACING AND SHORING FOR THE CONSTRUCTION LOADING CONDITIONS AND STABILITY OF THE STRUCTURE DURING CONSTRUCTION. CONSTRUCTION LOADS SHALL NOT EXCEED DESIGN LOADS.
9. THE CONTRACTOR SHALL RETAIN A PROFESSIONAL ENGINEER REGISTERED IN THE PROVINCE OF BRITISH COLUMBIA TO DESIGN AND TAKE RESPONSIBILITY FOR ANY TEMPORARY SHORING, BRACING OR OTHER DESIGNS REQUIRED TO COMPLETE CONSTRUCTION.
10. THE CONTRACTOR SHALL SUBMIT WRITTEN PROCEDURES FOR CONCRETE WORK PERFORMED DURING COLD (BELOW +5°C) AND HOT (ABOVE +25°C) WEATHER. THE PROCEDURES SHALL BE SEALED AND SIGNED BY A PROFESSIONAL ENGINEER REGISTERED IN THE PROVINCE OF BRITISH COLUMBIA.
11. UNDER NO CIRCUMSTANCES SHALL DRAWINGS BE SCALED. COMPONENTS MAY BE SHOWN SCHEMATICALLY.
12. CONTRACTOR AND ALL SUB-TRADES SHALL VERIFY ALL DIMENSIONS ON SITE PRIOR TO COMMENCING FABRICATION.
13. ENVIRONMENTAL WORK PROCEDURES, TIMING, AND SPECIAL PRECAUTIONS SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS AND LIMITATIONS OF THE FEDERAL DEPARTMENT OF FISHERIES AND OCEANS, AND THE PROVINCIAL MINISTRY OF ENVIRONMENT.

SUBMITTALS

1. WHERE SHOP DRAWINGS ARE REQUESTED IN THE GENERAL NOTES THE CONTRACTOR SHALL PROVIDE THEM IN EITHER HARD COPY OR DIGITAL FORMAT TO THE FOLLOWING REQUIREMENTS FOR THE ENGINEER'S REVIEW PRIOR TO FABRICATION. THE SHOP DRAWINGS SHALL INDICATE DETAILS, DIMENSIONS, MATERIALS AND DESIGN LOADS.

2. IF HARD COPY FORMAT IS USED TWO PAPER COPIES SHALL BE SUBMITTED. UNLESS NOTED OTHERWISE THEY SHALL BE SIGNED AND SEALED BY A SPECIALTY ENGINEER REGISTERED IN THE PROVINCE OF BRITISH COLUMBIA.

3. DRAWINGS NOT SEALED BY THE SPECIALTY ENGINEER SHALL BE ACCOMPANIED BY A LETTER WITH A DRAWING LIST IDENTIFYING ALL DRAWING NUMBERS, TITLES, MOST RECENT REVISION NUMBERS AND DATES. THE LETTER AND DRAWING LIST ARE TO BE SIGNED AND SEALED BY THE SPECIALTY ENGINEER.

4. IF A DIGITAL SUBMISSION IS MADE THE FILES SHALL BE IN PDF FORMAT ON A DISC OR TRANSMITTED VIA E-MAIL. THE SUBMISSION SHALL CONTAIN A LETTER WITH A DRAWING LIST AS DESCRIBED ABOVE SIGNED AND SEALED BY THE SPECIALTY ENGINEER. THE FINAL SUBMISSION SHALL BE MADE AS A HARD COPY BEARING THE ORIGINAL SEAL AND SIGNATURE OF THE SPECIALTY ENGINEER REGISTERED IN THE PROVINCE OF BRITISH COLUMBIA.

5. THE FOLLOWING SUBMISSIONS ARE REQUIRED FOR THIS PROJECT:

- CONCRETE MIX DESIGNS
- REINFORCING BAR MILL CERTIFICATES (IF REQUESTED)
- WELDABLE REINFORCING BAR MILL CERTIFICATES (IF REQUESTED)
- EPOXY REINFORCING BAR PERFORMANCE TEST CERTIFICATES IF REQUESTED
- STRUCTURAL STEEL MILL CERTIFICATES IF REQUESTED
- REINFORCEMENT SHOP DRAWINGS
- PRE-CAST CONCRETE PANEL SHOP AND ERECTION DRAWINGS*
- STRUCTURAL STEEL SHOP DRAWINGS
- MISCELLANEOUS METAL FABRICATIONS
- NON-DESTRUCTIVE TEST (NDT) WELD TEST REPORTS
- OTHER SUBMISSIONS LISTED IN THE STANDARD SPECIFICATIONS

* INDICATES THE REQUIREMENT THAT SUBMISSION BE SEALED BY A SPECIALTY ENGINEER REGISTERED IN THE PROVINCE OF BRITISH COLUMBIA.

6. SHOP DRAWINGS WILL BE REVIEWED ONLY FOR GENERAL CONFORMITY WITH THE PROJECT DRAWINGS AND SPECIFICATIONS. QUANTITIES AND DETAILED DIMENSIONS ARE THE CONTRACTORS RESPONSIBILITY. THE REVIEW SHALL NOT RELIEVE THE CONTRACTOR FROM COMPLYING WITH ALL THE REQUIREMENTS OF THE CONTRACT DOCUMENTS INCLUDING COORDINATION WITH OTHER TRADES AND DISCIPLINES. THE CONTRACTOR IS RESPONSIBLE FOR ERRORS AND OMISSIONS ON THE SHOP DRAWINGS.

7. THE QUALITY CONTROL FOR MATERIALS, FABRICATION AND INSTALLATION IS THE RESPONSIBILITY OF THE CONTRACTOR AND SPECIALTY ENGINEERS.

MISCELLANEOUS METAL FABRICATIONS

1. MISCELLANEOUS METAL FABRICATIONS INCLUDES SUCH ITEMS AS METAL STAIRS AND LADDERS, PEDESTRIAN/BIKE GUARDS, VEHICLE GUARDS, PIPE AND UTILITY HANGERS ETC.

2. THE METAL FABRICATOR SHALL SUBMIT SHOP DRAWINGS AS SPECIFIED UNDER SUBMITTALS TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO FABRICATION. SHOP DRAWINGS SHALL INDICATE ALL DETAILS, MATERIAL SPECIFICATIONS, FINISHES AND DESIGN LOADS.

3. A COPY OF THE FABRICATOR'S CANADIAN WELDING BUREAU CERTIFICATES SHALL BE INCLUDED WITH THE SHOP DRAWING SUBMISSION.

4. ALL WELDING SHALL BE IN ACCORDANCE WITH CSA W59. FABRICATING SHOP TO HAVE A MINIMUM DIVISION 2.1 CERTIFICATION BY THE CANADIAN WELDING BUREAU TO THE REQUIREMENTS OF CSA W47.1. THE FABRICATOR SHALL SUBMIT PROOF OF CERTIFICATION PRIOR TO START OF WORK.

5. PROVIDE MATERIALS TO THE FOLLOWING STANDARDS:

- STEEL SECTIONS TO CAN/CSA-G40.21 GRADE 300W
- STEEL PLATE TO CAN/CSA-G40.21 GRADE 300W
- STEEL PIPE TO ASTM-A53/A53M, STANDARD WEIGHT, SCHEDULE 40, SEAMLESS, BLACK.
- METAL BAR GRATING TO ANSI/NAAMM MBG 531
- WELDING MATERIALS TO CSA W59
- FILLER METALS AND ALLIED MATERIALS FOR METAL ARC WELDING TO CSA W48
- ERECTION BOLTS TO ASTM A325
- ANCHOR BOLT/BRACKET ANCHORS TO ASTM F1554, GRADE 36 (36 ksi YIELD STRENGTH) OR ASTM A193 GRADE "B7"
- EPOXY GROUT BED UNDER GUARDRAIL BRACKETS SHALL BE NON-SHRINK, NON-METALLIC, FLOWABLE, 15MPa AFTER 24 HOURS.

6. PROVIDE 9.5 DIA DRAIN HOLE AT THE LOW POINT OF ALL HSS SECTIONS.

7. FABRICATE WORK SQUARE, PLUMB, STRAIGHT AND ACCURATE TO THE REQUIRED SIZES WITH JOINTS CLOSELY FITTED AND PROPERLY SECURED. WHERE POSSIBLE SHOP FIT AND ASSEMBLE READY FOR ERECTION. EXPOSED WELDS ARE TO BE CONTINUOUS FOR THE FULL LENGTH OF THE JOINT. GRIND SMOOTH AND FLUSH. UNLESS NOTED OTHERWISE USE SELF-TAPPING, SHAKE-PROOF, FLAT HEADED SCREWS ON ITEMS REQUIRING ASSEMBLY WITH SCREWS.

8. ISOLATE ALUMINUM FROM DISSIMILAR METALS EXCEPT ZINC OR WHITE BRONZE WITH BITUMINOUS PAINT OR OTHER APPLICABLE METHODS. ALL FASTENERS TO BE COMPATIBLE WITH THE MATERIALS THROUGH WHICH THEY PASS.

9. DELIVER, STORE, HANDLE AND PROTECT MATERIALS FROM DAMAGE. INSTALL PLUMB AND TRUE IN EXACT LOCATIONS, SECURELY FASTENED TO THE BUILDING STRUCTURE AS DETAILED.

FIELD REVIEWS

1. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A MINIMUM OF 24 HOURS (1 WORKING DAY) ADVANCE NOTICE FOR FIELD REVIEWS.

2. THE FOLLOWING FIELD REVIEWS ARE CONSIDERED TO BE THE MINIMUM NUMBER OF STRUCTURAL FIELD REVIEWS BY THE ENGINEER OF RECORD FOR THE PROJECT:

CONCRETE DECK/ABUTMENT : REINFORCING STEEL SHALL BE REVIEWED PRIOR TO PLACING CONCRETE. REINFORCING IN CONCRETE DECK/ABUTMENT SHALL BE REVIEWED PRIOR TO "BUTTONING UP" FORMS.

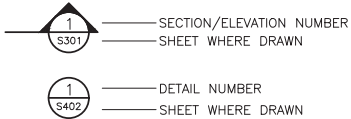
STRUCTURAL STEEL: SHALL BE REVIEWED IN SHOP AFTER THE MEMBERS HAVE BEEN FABRICATED AND ARE IN THEIR FINAL POSITION WITH ALL CONNECTIONS COMPLETE AND ALL BOLTS INSTALLED AND TIGHTENED.

METAL DECK: METAL DECK SHALL BE REVIEWED AFTER ALL SHEETS AND PERIMETER ANGLES ARE INSTALLED, FASTENING IS COMPLETE AND PRIOR TO COVERING.

IF THE ENGINEER OF RECORD IS NOT PROVIDED WITH THE OPPORTUNITY TO PERFORM THE REQUIRED FIELD REVIEWS, FINAL LETTERS OF ASSURANCE FOR THE PROJECT WILL NOT BE ISSUED

2. THE OWNER'S QUALITY ASSURANCE REPRESENTATIVE WILL PERFORM THE REMAINING FIELD REVIEWS FOR THE PROJECT AS DETAILED IN THE STANDARD SPECIFICATIONS, THESE NOTES AND THE GOVERNING CODES AND STANDARDS.

SYMBOLS AND ABBREVIATIONS



- CL.

℄

CP.

C/W

DWG.

EL.

REV.

I.D.

LLH

LLV

MAX.

MIN.

M.o.T.

N.T.S.

OPP.

PL

PROJ.

R

SIM.

S.S.

T.O.

TYP.

U/S

U.N.O.

WP
- CLEAR

— CENTRELINE

— COMPLETE PENETRATION

— COMPLETE WITH

— DRAWING

— ELEVATION

— REVISION

— INSIDE DIAMETER

— LONG LEG HORIZONTAL

— LONG LEG VERTICAL

— MAXIMUM

— MINIMUM

— MINISTRY OF TRANSPORTATION AND INFRASTRUCTURE

— NOT TO SCALE

— OPPOSITE

— PLATE

— PROJECTION

— RADIUS

— SIMILAR

— STAINLESS STEEL

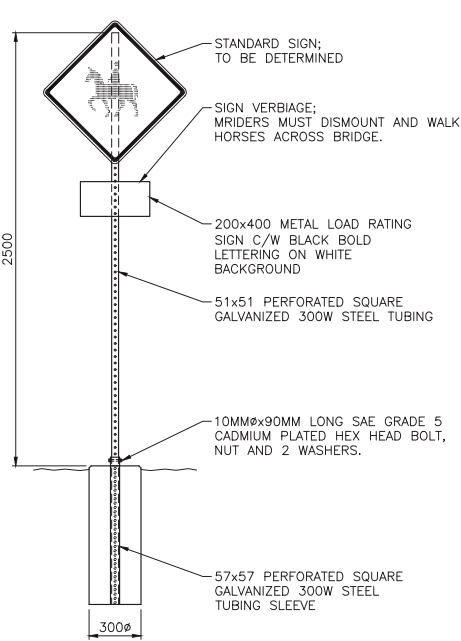
— TOP OF

— TYPICAL

— UNDERSIDE

— UNLESS NOTED OTHERWISE

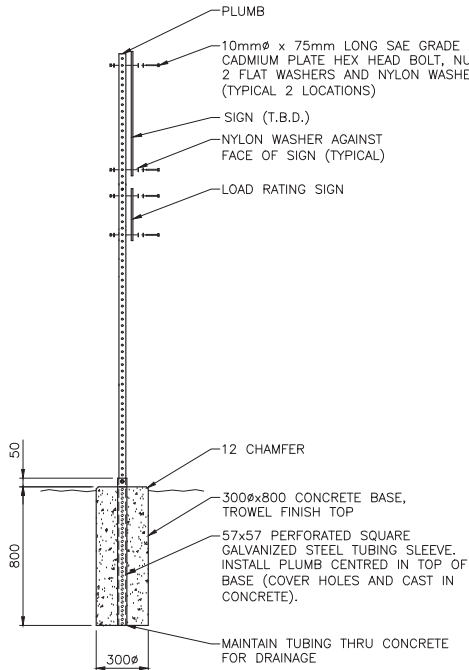
— WORK POINT



FRONT VIEW

SIGN POST DETAIL

N.T.S.
NOTE: PROVIDE 1-SIGN POST AT EACH END OF BRIDGE.



SIDE VIEW

SIGN POST DETAIL

N.T.S.

ISSUED FOR REVIEW

NOT FOR
CONSTRUCTION

ISSUES						SUB CONSULTANT	<div><div></div><div>HEROLD ENGINEERING</div></div> <div>3701 Sherton Rd, Nanaimo, BC V9T 2H1 Tel: 250-751-8558 Fax: 250-751-8559 Email: mail@heroldengineering.com</div>	ENGINEERS SEAL	GENERAL NOTES SHEET 1	LITTLE QUALICUM RIVER PEDESTRIAN BRIDGE 6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2 REGIONAL DISTRICT OF NANAIMO	HEL PROJECT No.	CLIENT DWG. No.
No.	DATE	ISSUED FOR	No.	DATE	ISSUED FOR						0837-053	N/A
A	2018.11.09	CLIENT REVIEW										
B	2019.01.11	CLIENT REVIEW										
© Copyright reserved. This drawing remains the exclusive property of Herold Engineering Limited and may not be reused or reproduced without written consent of Herold Engineering Limited.											SCALE	PERMIT No.
											AS SHOWN	N/A
											HEL DRAWING No.	REVISION
											S01	B

PROJECT: 03-052 LRS, QUALICUM RIVER BRIDGE, REGIONAL DISTRICT OF NANAIMO, BC V9T 6N2
DATE: 2018.11.09
DRAWN BY: J.S.J.
CHECKED BY: J.S.J.
APPROVED BY: J.S.J.
PROJECT: 03-052 LRS, QUALICUM RIVER BRIDGE, REGIONAL DISTRICT OF NANAIMO, BC V9T 6N2
DATE: 2018.11.09
DRAWN BY: J.S.J.
CHECKED BY: J.S.J.
APPROVED BY: J.S.J.

PILE FOUNDATIONS: STEEL PIPE

- REFER TO THE GEOTECHNICAL REPORT PREPARED BY EBA TETRATECH. FILE # VGE003532-01
- STEEL PILES SHALL CONFORM TO ASTM A252, GRADE 2 OR BETTER. SPLICES AND SEAMS SHALL BE FABRICATED WITH FULL PENETRATION BUTT WELDS IN ACCORDANCE WITH CSA W59. MILL CERTIFICATES FOR THE PILE MATERIAL SHALL BE SUBMITTED TO THE ENGINEER. PREVIOUSLY USED PIPE WILL NOT BE ACCEPTED.
- REINFORCING STEEL SHALL MEET THE REQUIREMENTS OF CSA A23.1 AND CAN/CSA-G30.18 FOR GRADE 400 REINFORCING.
- CAST-IN-PLACE CONCRETE AND CONSTITUENT MATERIALS FOR PILE FILL SHALL MEET THE REQUIREMENTS OF CSA A23.1. THE COMPRESSIVE STRENGTH OF CONCRETE f'c SHALL BE DETERMINED BY TESTING AS SPECIFIED IN CSA A23.1 AND CSA A23.2. CONCRETE COMPRESSIVE STRENGTH IS SPECIFIED ON THE STRUCTURAL DRAWINGS.
- PILES SHALL BE INSTALLED IN THE LOCATIONS SHOWN ON THE STRUCTURAL DRAWINGS. THE CONTRACTOR IS RESPONSIBLE FOR LAYOUT OF THE PILES. WHERE CONCRETE OR OTHER OBSTRUCTIONS ARE ENCOUNTERED THEY SHALL BE REMOVED.
- PILE DRIVING EQUIPMENT SHALL BE SUFFICIENT TO DELIVER FORCE TO DRIVE THE PILE TO END BEARING. PRACTICAL REFUSAL ON THE DENSE NATIVE SANDS AND GRAVEL OR TILL AS PER THE GEOTECHNICAL REPORT HEREIN ATTACHED. THE GEOTECHNICAL ENGINEER WILL ESTABLISH THE FINAL TIP ELEVATIONS BASED ON THE DESIGN LOADS, RESISTANCE TO PILE DRIVING, PILE TYPE AND METHOD OF DRIVING.
- THE CONTRACTOR IS TO SUBMIT WITH THE BID THE METHOD OF INSTALLATION COMPLETE WITH THE METHOD OF CERTIFYING ACCEPTANCE THAT THE PILE MEETS THE ULS LOAD CRITERIA OF 675kN PER PILE. THE ACCEPTANCE CRITERIA ARE DISCUSSED IN THE GEOTECHNICAL REPORT.
- DROP HAMMER AND DIESEL HAMMER ARE CONSIDERED ACCEPTABLE METHODS. VIBRATORY METHODS ARE NOT ACCEPTABLE FOR THIS PROJECT EXCEPT FOR USE IN EXTRACTION, IF REQUIRED.
- ALL PILES SHALL BE DRIVEN TO DEPTHS INDICATED IN THE GEOTECHNICAL REPORT TO THE SATISFACTION OF THE GEOTECHNICAL ENGINEER.
- MAXIMUM TOLERANCES FROM THE LOCATIONS SHOWN ON THE DRAWINGS SHALL BE 75mm IN ANY DIRECTION AT THE POINT OF CUT-OFF AS INDICATED ON THE DRAWINGS. MAXIMUM DEVIATION FROM PLUMB BELOW THE GROUND LINE IS 5mm PER METRE.
- PILES SHALL BE DRIVEN OPEN ENDED, UNLESS SPECIFIED OTHERWISE, TO PRACTICAL REFUSAL. ADJACENT PILES ARE TO BE MONITORED FOR UPLIFT DUE TO THE DRIVING PROCEDURE AND ARE TO BE RE-DRIVEN TO REFUSAL UPLIFT OCCURS. ONCE THE PILE IS ACCEPTED BY THE GEOTECHNICAL ENGINEER IT IS TO BE CLEANED OUT OF ALL SOIL MATERIAL AND WATER TO THE BOTTOM. THE AREA AROUND THE PILE IS TO BE EXCAVATED TO THE LEVEL INDICATED ON THE DRAWINGS. REINFORCEMENT IS TO BE PLACED AS SHOWN ON THE DRAWINGS AND THE PILE FILLED WITH CONCRETE IN ACCORDANCE WITH CSA A23.1. DEWATERING AND CONTROL OF WATER DURING THE FILLING PROCESS IS THE RESPONSIBILITY OF THE CONTRACTOR.
- THE TOP OF THE PILES SHALL BE PROTECTED BY A SUITABLE DRIVING CAP TO PREVENT DAMAGE TO THE PILES. PILES SHALL BE DRIVEN WITHOUT EXCESSIVE DEFORMATION TO THEIR HEADS. SUFFICIENT LENGTH OF PILE ABOVE CUT-OFF SHALL BE ALLOWED SO THAT NO PART OF THE HEAD OF THE PILE DAMAGED DURING INSTALLATION REMAINS IN THE WORK. DAMAGED PILES SHALL BE REPAIRED OR REPLACED AS DIRECTED BY THE GEOTECHNICAL ENGINEER.
- ANY PILE SO DAMAGED AS TO BE UNFIT FOR THE USE FOR WHICH IT WAS INTENDED, AND ANY PILE WHICH CANNOT BE BROUGHT WITHIN TOLERANCE FOR LOCATION WILL BE REJECTED. A REJECTED PILE SHALL BE RETRACTED AND REPLACED BY A NEW PILE. WHERE A PILE CANNOT BE RETRACTED OR THE GEOTECHNICAL ENGINEER DOES NOT APPROVE THE RETRACTION, THEN A NEW PILE IS TO BE DRIVEN IN A LOCATION AS DIRECTED BY THE STRUCTURAL ENGINEER OF RECORD. ALL COSTS ASSOCIATED WITH REJECTED PILES SHALL BE PAID BY THE CONTRACTOR WITH NO ADDITIONAL COSTS TO THE OWNER.
- ALL STEEL PIPE PILES SHALL BE CUT-OFF AT THE ELEVATION INDICATED, REINFORCED AND CONCRETE FILLED AND TEMPORARILY PROTECTED BY EFFECTIVE MEANS. REINFORCING STEEL PROTRUDING FROM THE TOP OF THE PILE IS TO BE FLAGGED FOR VISIBILITY REASONS.
- PILES ARE EXPECTED TO BE SPLICED NO MORE THAN ONCE PER PILE AND THE BID PRICE IS TO INCLUDE A SINGLE SPLICE. WHEN CONDITIONS OCCUR WHERE AN EXTRA SPLICE IS REQUIRED, PILING SHALL BE SPLICED A MAXIMUM OF TWICE PER PILE. THE MINIMUM LENGTH OF PILE BETWEEN SPLICES SHALL BE 8 METRES. PILING SHALL BE ALIGNED PLUMB AND STRAIGHT END TO END. ALL WELDS SHALL BE PRE-QUALIFIED FULL PENETRATION BUTT WELDS IN ACCORDANCE WITH CSA W59 AND THE CANADIAN WELDING BUREAU. WELDS AND WELDING PROCESSES SHALL BE PROTECTED FROM ENVIRONMENTAL ELEMENTS IN ACCORDANCE WITH THE BEST TRADE PRACTICE. PREHEAT AND INTERPASS TEMPERATURES ARE TO MEET STANDARDS SPECIFIED THEREIN FOR COLD WEATHER FIELD WELDING. WELDING SHALL BE DONE BY WELDING FIRMS QUALIFIED AS CSA W47.1 DIVISION 2.1 OR BETTER. THE CONTRACTOR SHALL PRODUCE EVIDENCE THAT ALL WELDING OPERATORS ARE CURRENTLY QUALIFIED TO THE STANDARDS HEREIN REQUIRED. SUBMIT SHOP DRAWINGS SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE PROVINCE OF BRITISH COLUMBIA SHOWING WELDED SPLICE DETAILS.

MECHANICAL AND ADHESIVE ANCHORS

- ALL ANCHORS ARE TO BE INSTALLED IN STRICT ACCORDANCE WITH THE MANUFACTURER'S WRITTEN INSTRUCTIONS.
- ALL ANCHORS ARE TO BE THE ADHESIVE TYPE. MECHANICAL ANCHORS ARE ONLY TO BE USED WHEN SPECIFICALLY CALLED-UP ON THE DRAWINGS. SUBSTITUTIONS MUST BE APPROVED BY THE PROJECT ENGINEER PRIOR TO USE.
- UNLESS NOTED OTHERWISE ADHESIVE ANCHORS SHALL BE HILTI 'HAS-E' OR 'HIT-Z' ROD. REFER TO DRAWINGS FOR ANCHOR LOCATIONS, SIZES, CENTRES AND EMBEDMENT LENGTH.

USE HILTI HIT-HY200 WHEN:
A QUICK CURE IS REQUIRED,
CONDITIONS ARE DRY,
HOLES ARE HAMMER DRILLED,
HOLES ARE NOT OVER-SIZED,
BASE MATERIAL TEMPERATURE IS ABOVE MINUS 10° CELCIUS.

USE HILTI HIT-RE500-V3 WHEN:
EXTENDED WORKING TIME IS REQUIRED AND CURE TIME IS NOT CRITICAL,
HOLES ARE DRILLED USING DIAMOND CORE, PNEUMATIC OR HAMMER DRILLS,
DEEP EMBEDMENT IS SPECIFIED,
THE APPLICATION IS UNDERWATER, OR
HOLES ARE OVERSIZED.
- REFER TO DRAWINGS FOR MECHANICAL ANCHOR LOCATIONS, SIZES, CENTRES AND EMBEDMENT LENGTH.
- HOLES FOR MECHANICAL ANCHORS SHALL BE CLEANED OUT WITH HIGH PRESSURE AIR OR BRUSH PRIOR TO ANCHOR INSTALLATION.
- INSTALLERS OF HILTI PRODUCTS SHALL HAVE RECEIVED TRAINING BY HILTI (CANADA) CORP. IN THE USE OF THE SPECIFIED PRODUCTS. THE GENERAL CONTRACTOR SHALL PROVIDE THE DESIGN ENGINEER WITH A LETTER STATING THAT THIS TRAINING HAS BEEN COMPLETED.

STRUCTURAL STEEL

- ALL STEEL WORK SHALL BE IN ACCORDANCE WITH CSA-S6, THE STANDARD SPECIFICATIONS AND THE REVIEWED SHOP DRAWINGS.
- THE STEEL FABRICATOR SHALL SUBMIT SHOP DRAWINGS AS SPECIFIED UNDER SUBMITTALS TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO FABRICATION. SHOP DRAWINGS SHALL INDICATE ALL DETAILS, FASTENERS, MATERIAL SPECIFICATIONS, FINISHES AND DESIGN LOADS.
- A COPY OF THE FABRICATOR'S CANADIAN WELDING BUREAU CERTIFICATES SHALL BE INCLUDED WITH THE SHOP DRAWING SUBMISSION.
- ALL WELDING SHALL BE IN ACCORDANCE WITH CSA W59 AND SHALL BE PERFORMED BY FABRICATORS QUALIFIED BY THE CANADIAN WELDING BUREAU UNDER CSA W55.3. FABRICATING SHOP TO HAVE A MINIMUM DIVISION 2.1 CERTIFICATION BY THE CANADIAN WELDING BUREAU TO THE REQUIREMENTS OF CSA W47.1 AND CSA W55.3 FOR RESISTANCE WELDING OF STRUCTURAL COMPONENTS. THE FABRICATOR SHALL SUBMIT PROOF OF CERTIFICATION PRIOR TO START OF WORK.
- ALL WELDING ELECTRODES SHALL CONFORM TO CSA W48.
- CONNECTIONS NOT DETAILED ON THE STRUCTURAL DRAWINGS SHALL BE DESIGNED FOR THE LOADS INDICATED ON THE DRAWINGS. UNLESS NOTED OTHERWISE ON THE STRUCTURAL DRAWINGS CONNECTION DETAILS ARE SCHEMATIC ONLY AND FINAL CONNECTION CONFIGURATION IS THE RESPONSIBILITY OF THE FABRICATOR. USE A MINIMUM OF 2-M20 (3/4") A325 BOLTS PER CONNECTION. CONNECTIONS DESIGNED BY THE CONTRACTOR SHALL BE SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE PROVINCE OF BRITISH COLUMBIA. CONNECTIONS SHALL BE DESIGNED TO CSA-S16-09 TO RESIST FORCES, MOMENTS AND SHEARS INDICATED ON THE PLANS. IN INSTANCES OF NON-COMPLIANCE THE FABRICATOR SHALL BE RESPONSIBLE FOR ADDITIONAL COSTS ASSOCIATED WITH ACHIEVING COMPLIANCE WITH THE STANDARD.
- CONNECTIONS DETAILED ON THESE STRUCTURAL DRAWINGS SHALL BE FABRICATED AND ERECTED AS SHOWN. ALTERNATIVES MAY BE CONSIDERED AT THE SOLE DISCRETION OF THE STRUCTURAL ENGINEER OF RECORD BUT MUST BE PRE-APPROVED BY SAME. WHERE AN ALTERNATIVE IS APPROVED IT SHALL BE AT NO ADDITIONAL COST TO THE OWNER AND SHALL NOT NEGATIVELY IMPACT THE CONSTRUCTION SCHEDULE.
- FABRICATOR SHALL INCREASE WELD SIZES TO ACCOMMODATE SLOT WIDTHS SO THAT LEG SIZE AS SPECIFIED IS FULLY ON STEEL CONNECTION ELEMENT. FINAL WELD SIZE TO BE SHOWN ON THE SHOP DRAWINGS.
- BOLTS AND ANCHOR RODS SHALL BE LONG ENOUGH THAT THE END OF THE BOLT OR ROD IS OUTSIDE THE FACE OF THE NUT.
- ALL WELDED, HEADED STUDS, AND WELDED DEFORMED BAR ANCHORS SHALL BE INSTALLED AS PER THE MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS IN ACCORDANCE WITH CSA W55.3.
- DO NOT FIELD BURN BASE PLATE HOLES OR CONNECTION BOLT HOLES UNLESS APPROVED IN WRITING BY THE ENGINEER. NO FIELD CUTTING OR ALTERATION OF STRUCTURAL MEMBERS IS TO OCCUR WITHOUT THE PRIOR WRITTEN APPROVAL OF THE ENGINEER
- IF ANCHOR BOLTS ARE MISPLACED OR BOLT HOLES MISALIGNED, INFORM THE ENGINEER.

STEEL GIRDERS	350A
ITEMS WELDED DIRECTLY TO GIRDERS	350A
ITEMS IDENTIFIED AS FRACTURE CRITICAL	350AT CATEGORY 2 OR BETTER
PIPE RAILINGS	240W (35W) TO ASTM A53, GALVANIZED
WIDE FLANGE SECTIONS	350W (50W) OR ASTM A992/A992M-11, GALV.
CHANNELS AND ANGLES	300A, OR 300W (44W) GALV
HSS SECTIONS	350W (50W) CLASS 'C', GALV
MISCELLANEOUS STEEL PLATES	300W (44W)
STUDS	ASTM A108, GRADE 1020

- BOLT SIZING SHALL BE AS NOTED ON DRAWINGS AND DETAILS. PROVIDE BOLTS TO CSA G40.21 WITH THE FOLLOWING GRADES:

ERECTION BOLTS TO ASTM A325-10
FINAL BOLTED CONNECTIONS; ASTM A325 UNLESS NOTED OTHERWISE
ANCHOR RODS SHALL MEET THE REQUIREMENTS OF ASTM F1554, GRADE 105 OR A193 B7 (105 ksi YIELD STRENGTH) AND SHALL BE HOT DIPPED GALVANIZED PER ASTM A153 OR ZINC PLATED PER ASTM B695.
- ALL BOLTS USED IN CONNECTIONS SHALL BE PRE-TENSIONED AND HAVE SURFACES OF CLASS 'A' OR BETTER.
- THE CONTRACTOR SHALL PROVIDE SEAL WELDED CLOSURE PLATES AT ALL OPEN ENDS OF EXTERIOR HSS SECTIONS. PLATE THICKNESS SHALL BE A MINIMUM OF 6 mm (1/4") UNLESS NOTED OTHERWISE. PROVIDE 9.5 DIA DRAIN HOLE AT LOW END OF ALL HSS PIECES

WELDING INSPECTIONS

- ALL INSPECTIONS SHALL BE PERFORMED BY A COMPANY CERTIFIED TO CSA W178.1 AND EMPLOYING FIELD INSPECTORS CERTIFIED TO CSA W178.2. BOTH SHALL BE CERTIFIED FOR THE RELEVANT CLASS OF INSPECTION. INSPECTION PROCEDURES SHALL BE AS OUTLINED BELOW.
- ALL CP TENSION WELDS SHALL BE TESTED BY A THIRD PARTY WELDING INSPECTOR ENGAGED BY CONTRACTOR USING NON-DESTRUCTIVE MEANS (X-RAY OR ULTRASONIC). PROVIDE TEST REPORTS UPON REQUEST.
- ALL WELDS ARE TO BE VISUALLY INSPECTED BY CONTRACTOR OR BY CERTIFIED THIRD PARTY ENGAGED BY CONTRACTOR. PROVIDE WRITTEN REPORTS UPON REQUEST.
- AT IT'S DISRETION, OWNER MAY ENGAGE A THIRD PARTY WELD INSPECTOR (OWNER'S REPRESENTATIVE) TO PERFORM PERIODIC VISUAL REVIEW OF WELDS OR INDEPENDANT NON-DESTRUCTIVE TESTING, PER THE STANDARD SPECIFICATIONS. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING INSPECTIONS AND PROVIDING SUITABLE AND SAFE ACCESS TO THE WORK BEING INSPECTED.
- ALL FAILURES IDENTIFIED BY THE TESTING AND INSPECTIONS SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE. COST OF ADDITIONAL TESTING TO CONFIRM CONFORMANCE WITH SPECIFICATIONS SHALL BE BORNE BY THE CONTRACTOR.
- SUBMIT ALL TEST REPORTS TO THE STRUCTURAL ENGINEER FOR REVIEW. DO NOT COVER MEMBERS AND THEIR CONNECTIONS WITHOUT THE APPROVAL OF THE STRUCTURAL ENGINEER.

REINFORCING STEEL

- REINFORCING STEEL SHALL BE DEFORMED STEEL 400 GRADE AND SHALL CONFORM TO CAN/CSA-G30.18
- WELDABLE LOW ALLOY DEFORMED STEEL REINFORCING BARS, GRADE 400W, SHALL CONFORM TO CAN/CSA-G30.18. MILL CERTIFICATES SHALL BE SUPPLIED TO THE STRUCTURAL ENGINEER FOR ALL WELDABLE REINFORCING STEEL USED IN THE PROJECT.
- WELDED WIRE FABRIC, DEFORMED, SHALL CONFORM ASTM A497.
- WELDING OF REINFORCING STEEL SHALL CONFORM TO CSA W186 "WELDING OF REINFORCING BARS IN REINFORCED CONCRETE CONSTRUCTION". WELDING OF REINFORCING SHALL BE ALLOWED ONLY AS NOTED ON PLANS. WHERE WELDING OF REINFORCING IS REQUIRED MILL CERTIFICATES FOR WELDABLE REINFORCING SHALL BE PROVIDED PRIOR TO WELDING. WRITTEN PERMISSION FROM THE STRUCTURAL ENGINEER IS REQUIRED FOR ANY ADDITIONAL WELDING.
- ALL REINFORCING BARS SHALL BE TIED SECURELY TO PREVENT DISPLACEMENT.
- UNLESS NOTED OTHERWISE ON PLANS, LAP LENGTHS FOR REINFORCING STEEL SHALL BE AS FOLLOWS:

REINFORCING BAR LAP LENGTHS						
CONCRETE MPa	BAR SIZE					
	10M	15M	20M	25M	30M	35M
30	355 (14")	510 (20")	710 (28")	1065 (42")	1295 (51")	1500 (59")
35	330 (13")	480 (19")	660 (26")	990 (39")	1195 (47")	1395 (55")
40	305 (12")	455 (18")	610 (24")	940 (37")	1120 (44")	1320 (52")
45	280 (11")	430 (17")	560 (22")	890 (35")	1040 (41")	1245 (49")
NOTES: 1. MULTIPLY VALUES BY 1.3 FOR HORIZONTAL REINFORCEMENT PLACED IN SUCH A WAY THAT MORE THAN 12" OF FRESH CONCRETE IS CAST IN THE MEMBER BELOW THE SPLICE. 2. MULTIPLY VALUES BY 1.5 FOR EPOXY COATED REINFORCEMENT WITH CLEAR COVER LESS THAN 3 BAR DIAMETERS OR BAR SPACING LESS THAN 7 BAR DIAMETERS. 3. MULTIPLY VALUES BY 1.2 FOR ALL EPOXY COATED REINFORCEMENT OTHER THAN IN 2. ABOVE.						

- NO SPLICES OTHER THAN THOSE NOTED ON THE DRAWINGS ARE PERMITTED WITHOUT WRITTEN PERMISSION FROM THE STRUCTURAL ENGINEER.
- WHERE CONCRETE SURFACES ARE TO BE EXPOSED ONLY NON-CORROSIVE TYPE REINFORCING CHAIRS SHALL BE USED TO SUPPORT THE REINFORCING STEEL.
- DOWELS ARE TO BE TIED IN PLACE PRIOR TO POURING CONCRETE - "WET DOWELING" OF ANY REINFORCING STEEL IS NOT PERMITTED WITHOUT THE WRITTEN APPROVAL OF THE STRUCTURAL ENGINEER.
- HOOKS ON ALL TIES SHALL BE BENT AT LEAST 135° AND HAVE A MINIMUM LEG OF 6 TIMES THE TIE BAR DIAMETER.
- PROVIDE CORNER BARS TO MATCH HORIZONTAL WALL REINFORCEMENT.
- ALL BARS SHALL BE BENT AT TEMPERATURES GREATER THAN 10°C.
- NO BARS WHICH ARE PARTIALLY EMBEDDED IN CONCRETE SHALL BE FIELD BENT EXCEPT AS SHOWN ON THE DRAWINGS OR APPROVED IN WRITING BY THE PROJECT STRUCTURAL ENGINEER.

CAST-IN-PLACE CONCRETE

- HEL-014
- ALL CONCRETE WORK SHALL CONFORM TO THE REQUIREMENTS OF CAN/CSA A23.1-09 AND A23.2-09.
 - CONCRETE MIXES, AGGREGATES AND CEMENTITIOUS MATERIALS, INCLUDING PORTLAND CEMENT AND PORTLAND LESTONE CEMENT, SHALL CONFORM TO CAN/CSA A23.1-09 AND A23.2-09 AND CAN/CSA-A3000-08 AND SHALL HAVE THE FOLLOWING PROPERTIES BASED UPON PERFORMANCE CRITERIA PROPORTIONING:

CLASS	28 DAY STRENGTH	MAX. AGG. SIZE	SLUMP	AIR CONTENT	EXPOSURE	CEMENT TYPE
ABUTMENTS & PILE FILL	30MPa	19 mm (¾")	80mm ±20	4-7%	F-2	GU
BRIDGE DECK	45 MPa	19 mm (¾")	80mm ±20	4-7%	F-2	GU
 - PORTLAND LESTONE CEMENT (PLC) SHALL MEET THE REQUIREMENTS OF CSA A3000 FOR LESTONE CEMENTS.
 - CONCRETE TESTING SHALL BE CARRIED OUT BY A THIRD PARTY TESTING FIRM ENGAGED BY THE CONTRACTOR AND PAID FOR BY THE OWNER AND SHALL BE IN ACCORDANCE WITH CAN/CSA A23.1-09 AND A23.2-09. THE MINIMUM NUMBER OF TESTS PERFORMED SHALL BE AS PER CSA A23.2-09. ADDITIONAL TESTING SHALL BE PERFORMED AT THE DIRECTION OF THE STRUCTURAL ENGINEER. CONTRACTOR SHALL PROVIDE TESTING AGENCY WITH ADEQUATE NOTICE TO PROVIDE TESTING AS REQUIRED.
 - CHAMFER ALL EXPOSED EDGES OF CONCRETE WITH A 19mm (3/4") CHAMFER UNLESS NOTED OTHERWISE.
 - CONCRETE FINISHES SHALL BE IN ACCORDANCE WITH CAN/CSA A23.1-09 AND AS FOLLOWS UNLESS NOTED OTHERWISE:

U/S DECK; SIDES OF DECK CONCRETE BARRIERS TOP OF DECK;	STEEL FORM STEEL FORM STEEL FORM BROOM FINISH
---	--
 - ALL CONCRETE CURING SHALL BE IN ACCORDANCE WITH CAN/CSA A23.1-09. SPECIAL PRECAUTIONS SHALL BE TAKEN PER CSA A23.1 FOR PLACING AND CURING CONCRETE AT OR ABOVE 27° C AND AT OR BELOW 5° C.
 - UNLESS NOTED OTHERWISE, OR REQUIRED FOR FIRE RESISTANCE RATING, ALL REINFORCING STEEL SHALL HAVE THE FOLLOWING CLEAR COVER DISTANCES:

CAST AGAINST AND PERMANENTLY EXPOSED TO EARTH	75 mm
BRIDGE DECK U.N.O.	35 mm

* IN ADDITION, COVER MUST BE AT LEAST 1.0x THE BAR DIAMETER FOR INTERIOR EXPOSURE, AND 1.5x THE BAR DIAMETER FOR EXTERIOR EXPOSURE
 - CONTROL JOINTS SHALL BE PROVIDED IN BOTH DIRECTIONS IN ALL SLABS-ON-GRADE AT A MAXIMUM SPACING OF 3660mm (12'-0") FOR UNREINFORCED SLABS AND 6100mm (20'-0") FOR REINFORCED SLABS, UNLESS NOTED OTHERWISE ON DRAWINGS.
 - JOINT FILLER SHALL BE INSTALLED IN ALL EXPANSION AND CONSTRUCTION JOINTS.
 - EMBEDDED PLATES AND ANCHOR BOLTS FOR STRUCTURAL STEEL SHALL BE SECURELY TIED OR FASTENED IN PLACE PRIOR TO POURING CONCRETE. ALL ANCHOR BOLTS SHALL BE LAID OUT USING A TEMPLATE. "WET DOWELING" OF ANCHOR BOLTS AND EMBEDDED PLATES IS NOT PERMITTED.

ISSUED FOR REVIEW

NOT FOR CONSTRUCTION

ISSUES						SUB CONSULTANT	DRAFTED PHU	DRAFTING REVIEW	DESIGNED SJS	DESIGN REVIEW	HEROLD ENGINEERING 3701 Shenton Rd., Nanaimo, BC V9T 2H1 Tel: 250-751-8558 Fax: 250-751-8559 Email: mail@heroldengineering.com	ENGINEERS SEAL	GENERAL NOTES SHEET 2	LITTLE QUALICUM RIVER PEDESTRIAN BRIDGE 6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2 REGIONAL DISTRICT OF NANAIMO	HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A	SCALE AS SHOWN	PERMIT No. N/A	HEL DRAWING No. S02	REVISION B
No.	DATE	ISSUED FOR	No.	DATE	ISSUED FOR															
A	2018.11.09	CLIENT REVIEW																		
B	2019.01.11	CLIENT REVIEW																		
© Copyright reserved. This drawing remains the exclusive property of Herold Engineering Limited and may not be reused or reproduced without written consent of Herold Engineering Limited.																				

DESTROY ALL DRAWINGS SHOWING PREVIOUS REVISION



EXISTING SITE PLAN
1:250

ISSUED FOR REVIEW

NOT FOR
CONSTRUCTION

- NOTES:
1. FOR GENERAL NOTES, SEE DWG. S01 AND S02.
 2. THE LEGAL FABRIC IS COMPILED FROM OFFICIAL SOURCES (LISA, CLSR, CLR, ETC AS APPLICABLE). PLANS ARE CALCULATED AND ALIGNED TO BEST FIT FIELD TIED IP/MON'S, UNLESS OTHERWISE NOTED, THE BOUNDARIES SHOULD BE ACCURATE TO ACCEPTED STANDARDS AND USED ACCORDINGLY.

ISSUES					
No.	DATE	ISSUED FOR	No.	DATE	ISSUED FOR
A	2018.11.09	CLIENT REVIEW			
B	2019.01.11	CLIENT REVIEW			

SUB CONSULTANT

DRAFTED
PHU
DRAFTING REVIEW
DESIGNED
SJS
DESIGN REVIEW

HEROLD
ENGINEERING

3701 Shenton Rd., Nanaimo, BC V9T 2H1
Tel: 250-751-8558 Fax: 250-751-8559
Email: mail@heroldengineering.com

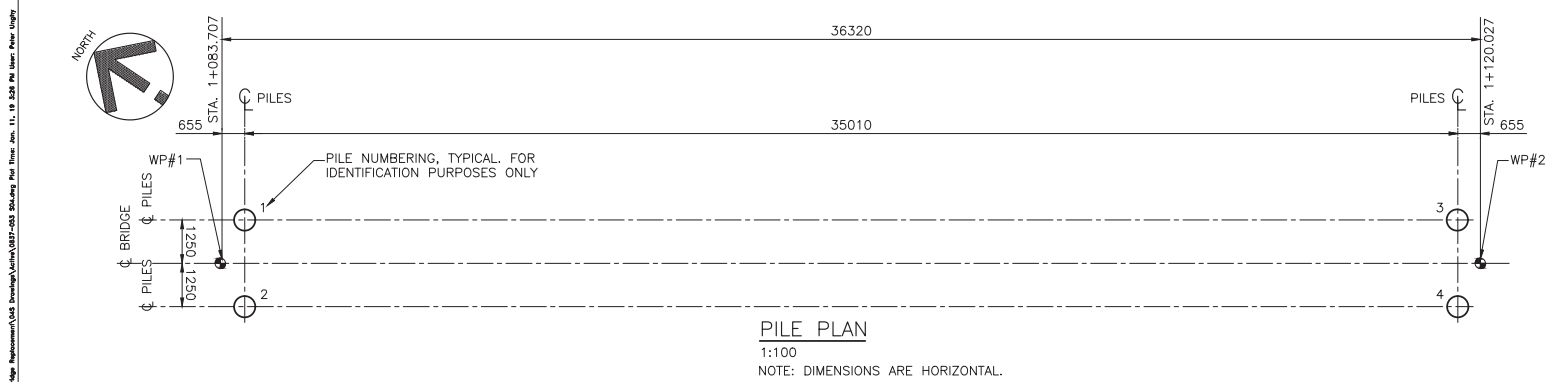
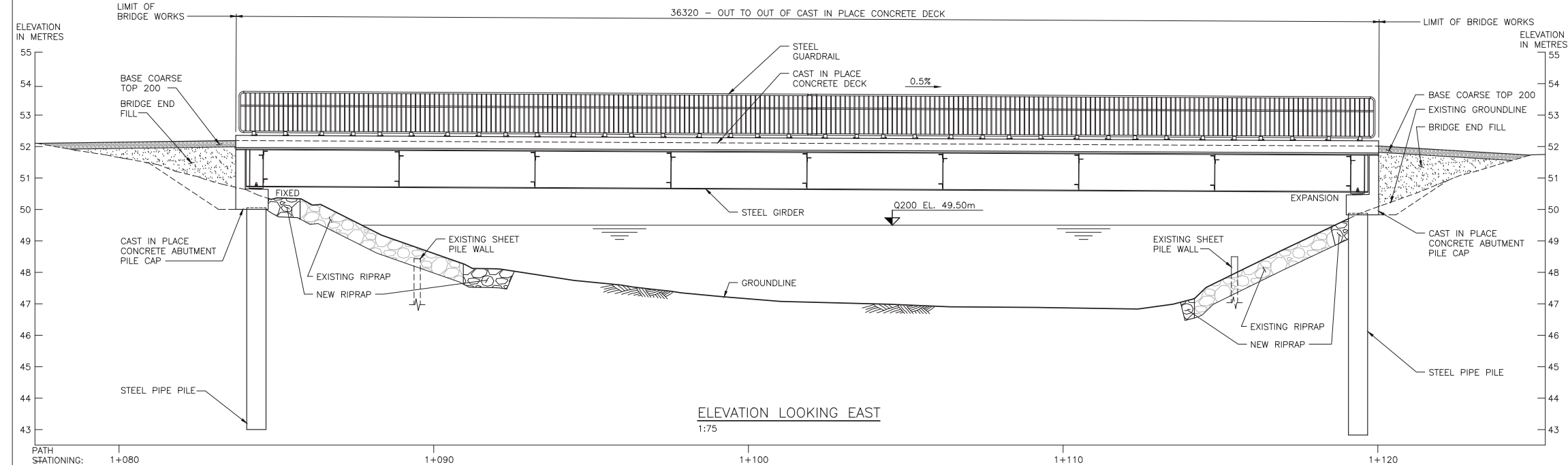
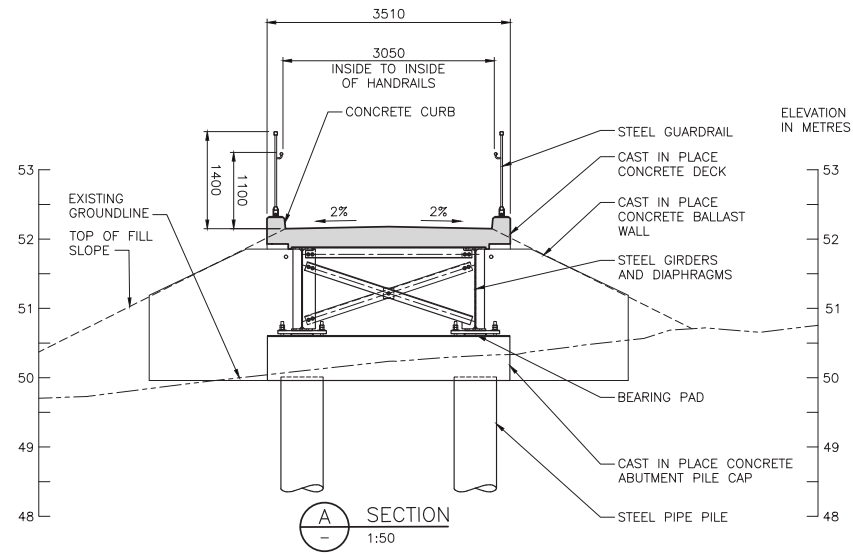
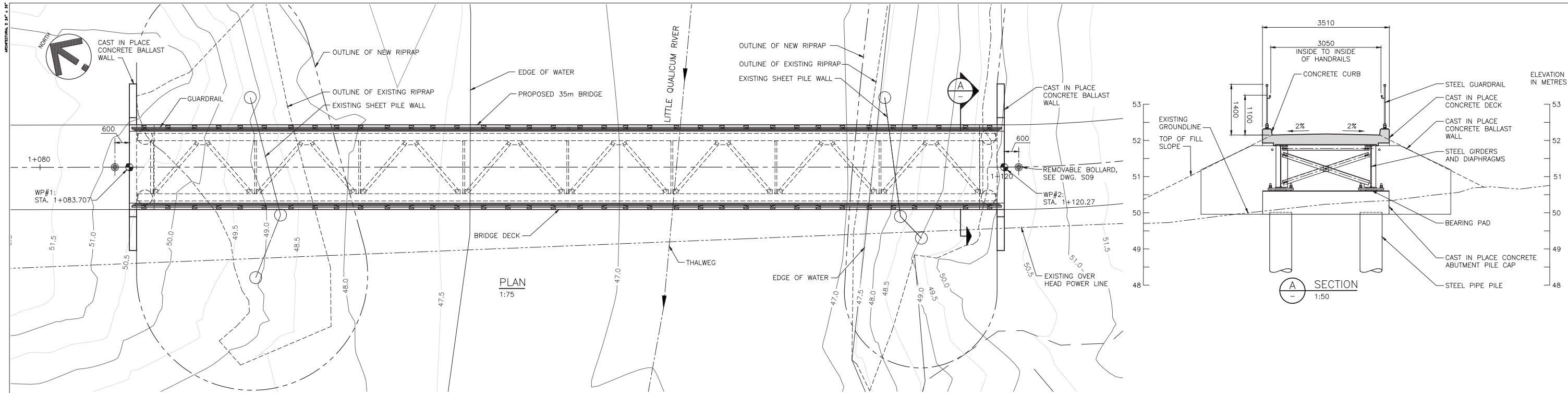
60

ENGINEERS SEAL

EXISTING SITE PLAN

LITTLE QUALICUM RIVER
PEDESTRIAN BRIDGE
6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2
REGIONAL DISTRICT OF NANAIMO

HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A
SCALE AS SHOWN	PERMIT No. N/A
HEL DRAWING No. S03	REVISION B



PILE LIST							
STRUCTURE	PILE #	SIZE: OUTSIDE DIAMETER AND WALL THICKNESS (mm)	CUT-OFF EL. (METRES)	ESTIMATED TIP EL. (METRES)	ADDITIONAL ALLOWANCE	ESTIMATED TOTAL LENGTH OF PILE (METRES)	AS BUILT LENGTH (METRES)
NORTH ABUTMENT	1	610 x 13	50.055	43.005	3.0	11.000	—
	2	610 x 13	50.055	43.005	3.0	11.000	—
SOUTH ABUTMENT	3	610 x 13	49.879	42.829	3.0	11.000	—
	4	610 x 13	49.879	42.829	3.0	11.000	—

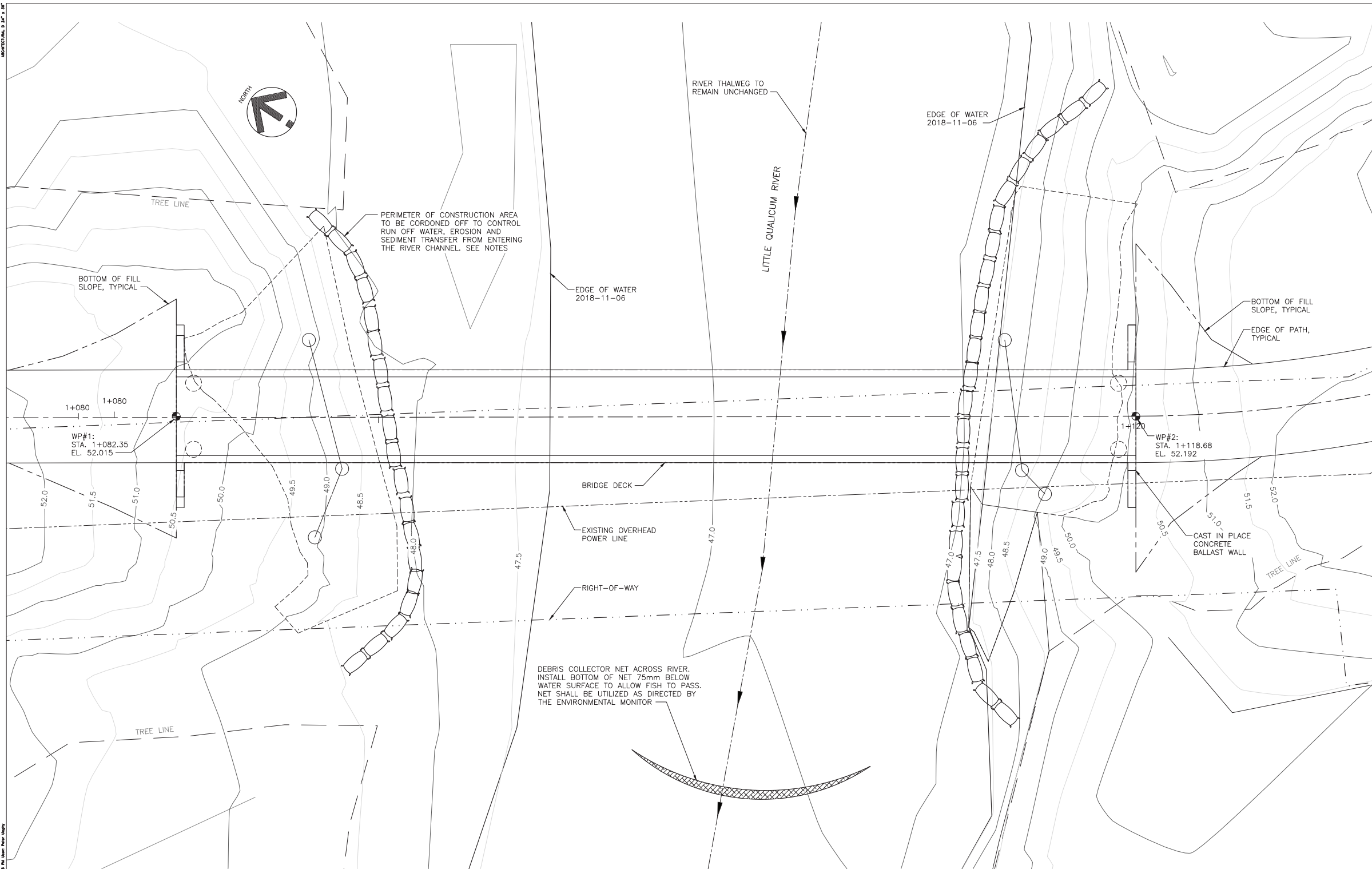
NOTE: FOR PILE TO PILE CAP CONNECTION, SEE DWG. S08

ISSUED FOR REVIEW

NOT FOR CONSTRUCTION

- NOTES:
- FOR GENERAL NOTES, SEE DWG. S01 AND S02.
 - WORKPOINTS (WP) ARE CENTRELINE OF BRIDGE/PATH, END OF CAST IN PLACE DECK.

ISSUES						SUB CONSULTANT	DRAFTED PHU	HEROLD ENGINEERING 3701 Shenton Rd, Nanaimo, BC V9T 2H1 Tel: 250-751-8558 Fax: 250-751-8559 Email: mail@heroldengineering.com	ENGINEERS SEAL	GENERAL ARRANGEMENT	LITTLE QUALICUM RIVER PEDESTRIAN BRIDGE 6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2 REGIONAL DISTRICT OF NANAIMO	HEL PROJECT No. 0837-053		CLIENT DWG. No. N/A							
ISSUED FOR												SCALE AS SHOWN		PERMIT No. N/A							
ISSUED FOR												HEL DRAWING No. S04		REVISION B							
ISSUED FOR												DESTROY ALL DRAWINGS SHOWING PREVIOUS REVISION									



ENVIRONMENTAL MITIGATION PLAN
1:75

NOTES:

ENVIRONMENTAL CONSTRUCTION REQUIREMENTS

- FOR GENERAL NOTES, SEE DWG. 0837-053 S01 AND 0837-053 S02.
- PERFORM WORKS IN ACCORDANCE WITH B.C. M.O.T.I. SS 165.
- ENVIRONMENTAL WORK PROCEDURES, TIMING, AND SPECIAL PRECAUTIONS SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS AND LIMITATIONS OF THE FEDERAL DEPARTMENT OF FISHERIES AND OCEANS (D.F.O.), AND THE PROVINCIAL MINISTRY OF FORESTS, LANDS, NATURAL RESOURCE OPERATIONS AND RURAL DEVELOPMENT.
- SECTION 11 NOTIFICATION UNDER THE BC WATER SUSTAINABILITY ACT AND IS THE RDN'S RESPONSIBILITY.
- EROSION AND SEDIMENT CONTROL FOR THIS PROJECT WILL BE AS OUTLINED IN THE FISHERIES AND OCEANS CANADA & MINISTRY OF WATER, LAND AND AIR PROTECTION (MWLAP) GUIDELINES, ENTITLED:
 - "LAND DEVELOPMENT GUIDELINES FOR THE PROTECTION OF AQUATIC HABITAT, SEPTEMBER 1993"
 - "ENVIRONMENTAL OBJECTIVES, BEST MANAGEMENT PRACTICES AND REQUIREMENTS FOR LAND DEVELOPMENT" - VANCOUVER ISLAND REGION, MARCH 2001
 - "ENVIRONMENTAL BEST MANAGEMENT PRACTICES FOR URBAN AND RURAL LAND DEVELOPMENT IN BRITISH COLUMBIA, JUNE 2004"
 - "EROSION AND SEDIMENT CONTROL GUIDELINE" BY THE CITY OF NANAIMO. IT IS INCUMBENT UPON THE CONTRACTOR TO ACQUIRE THESE GUIDELINES AND FOLLOW THE REQUIREMENTS THEREIN.
- ALL TREES WITHIN THE BRIDGE ALIGNMENT AND ANY TREES ADJACENT TO THE BRIDGE ALIGNMENT THAT ARE IN DANGER OF FALLING ARE TO BE FELLED AND REMOVED PRIOR TO APRIL 1. ANY TREE REMOVAL WORKS AFTER MARCH 1 SHALL BE PRECEDED BY A BIRD NEST SURVEY TO VERIFY THE ABSENCE OF EARLY NESTING SPECIES. TREE REMOVAL WORKS MUST AVOID DISTURBANCE TO THE RIVER CHANNEL AND BANKS - ADDITIONAL PRECAUTIONS MAY BE NEEDED TO ENSURE BANKS ARE NOT DISTURBED BY FALLING, BUCKING AND PERSONNEL ACCESS. ALL LOGS AND BRANCHES ARE TO BE COLLECTED AND REMOVED FROM THE SITE.
- PRIOR TO AND DURING CONSTRUCTION, THE CONTRACTOR SHALL TAKE FULL RESPONSIBILITY FOR CONTROLLING RUN-OFF WATER, EROSION AND SEDIMENT TRANSFER BY UTILIZING SUCH MEASURES AS CONSTRUCTION OF INTERCEPTOR DITCHES, SAND BAGS, SILT FENCES, HAY BALE STRUCTURES, SEDIMENT CONTROL PONDS, SEDIMENT TRAPS, STAGED GRAVEL FILTERS, OR OTHER METHODS HE MAY DEEM NECESSARY TO PREVENT DISCHARGE OF SEDIMENT INTO WATER COURSES. AN EROSION AND SEDIMENT CONTROL PLAN SHALL BE SUBMITTED FOR THE ENGINEER'S APPROVAL PRIOR TO CONSTRUCTION.
- UNLESS NOTED OTHERWISE, ALL TREES AND ROOTS SHALL BE PRESERVED WHERE POSSIBLE. ALL DISTURBED AREAS TO BE RESEED WITH NATIVE GRASS MIX. ANY SHRUBS REMOVED MUST BE REPLACED AND THE AREA IS TO BE COVERED IN STRAW MULCH UPON COMPLETION.
- ENVIRONMENTAL MONITOR SHALL BE PROVIDED BY THE REGIONAL DISTRICT OF NANAIMO.
- WITH THE EXCEPTION OF THE TREE REMOVAL WORKS, WHICH SHALL OCCUR PRIOR TO THE SONGBIRD BREEDING PERIOD, ALL BRIDGE CONSTRUCTION WILL OCCUR DURING THE REDUCED RISK TIMING WINDOW FOR VANCOUVER ISLAND (JUNE 15 THROUGH SEPTEMBER 15TH).
- ALL MACHINERY TO BE CLEAN, FREE OF LEAKS AND IN GOOD MECHANICAL CONDITION.
- REFUELLING OF ALL MACHINES TO BE DONE OUTSIDE OF THE 30 METER SETBACK.
- AS CEMENT AND CONCRETE ARE TOXIC TO AQUATIC ORGANISMS, ESPECIALLY FISH. ALL CONCRETE WORK MUST BE ISOLATED FROM WATER FOR A MINIMUM OF 48 HOURS. ALL EQUIPMENT SHALL BE CLEANED OFF SITE AWAY FROM STORM DRAINS.
- THE CONSULTANT AND CITY ASSUME NO RESPONSIBILITY FOR DAMAGES RESULTING FROM IMPROPER EROSION AND SEDIMENT CONTROL MEASURES UNDERTAKEN BY THE CONTRACTOR.
- SUBMIT ENVIRONMENTAL MITIGATION PLAN AT PRE-CONSTRUCTION MEETING.

ISSUED FOR REVIEW

NOT FOR
CONSTRUCTION

ISSUES					
No.	DATE	ISSUED FOR	No.	DATE	ISSUED FOR
A	2019.01.09	CO-ORDINATION			
B	2019.01.11	CLIENT REVIEW			

SUB CONSULTANT

DRAFTED
PHU
DRAFTING REVIEW
DESIGNED
SJS
DESIGN REVIEW

HEROLD
ENGINEERING

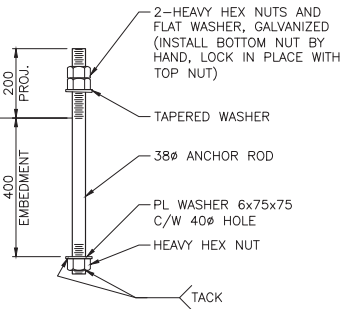
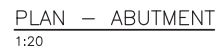
3701 Shenton Rd., Nanaimo, BC V9T 2H1
Tel: 250-751-8558 Fax: 250-751-8559
Email: mail@heroldengineering.com


ENGINEERS SEAL

ENVIRONMENTAL MITIGATION PLAN

LITTLE QUALICUM RIVER
PEDESTRIAN BRIDGE
6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2
REGIONAL DISTRICT OF NANAIMO

HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A
SCALE AS SHOWN	PERMIT No. N/A
HEL DRAWING No. S04	REVISION B




 DETAIL
 1:10
 NOTE: 4—REQUIRED FOR EACH
 ABUTMENT PILE CAP.



ISSUES							
No.	DATE	ISSUED FOR	No.	DATE	ISSUED FOR	No.	DATE
A	2018.11.09	CLIENT REVIEW					
B	2019.01.11	CLIENT REVIEW					

© Copyright reserved. This drawing remains the exclusive property of Hensel Engineering Limited and may not be reused or reproduced without written consent of Hensel Engineering Limited.

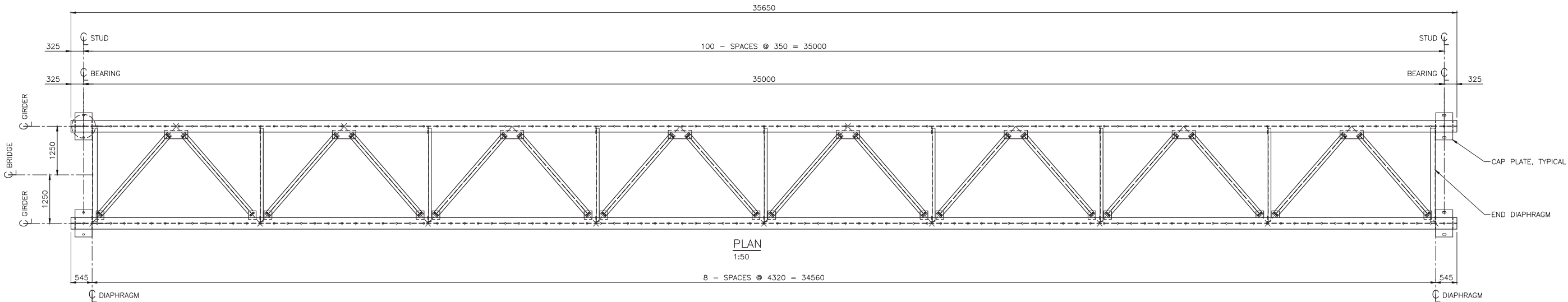
DRAFTED	PHU
DRAFTING REVIEW	
DESIGNED	SJS
DESIGN REVIEW	

CAST IN PLACE
ABUTMENTS

**LITTLE QUALICUM RIVER
PEDESTRIAN BRIDGE**
6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2
REGIONAL DISTRICT OF NANAIMO

HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A
SCALE AS SHOWN	PERMIT No. N/A
HEL DRAWING No. S06	REVISION B

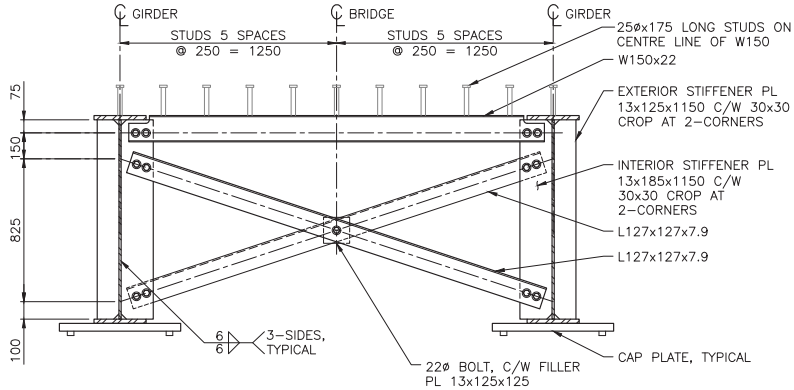
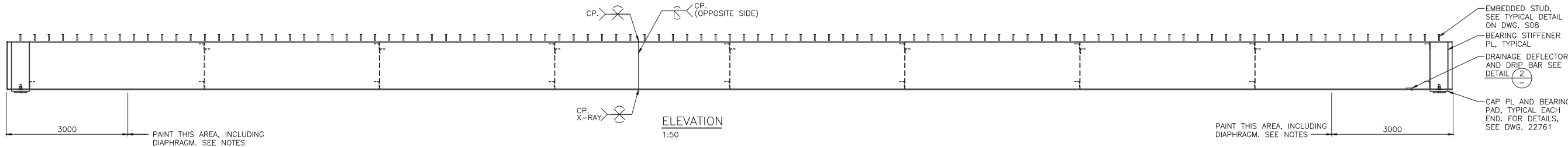
DESTROY ALL DRAWINGS SHOWING PREVIOUS REVISION



FABRICATE GIRDERS AS FRACTURE CRITICAL MEMBERS. TENSION FLANGE TO BE CHARPY TESTED TO CATEGORY 2.

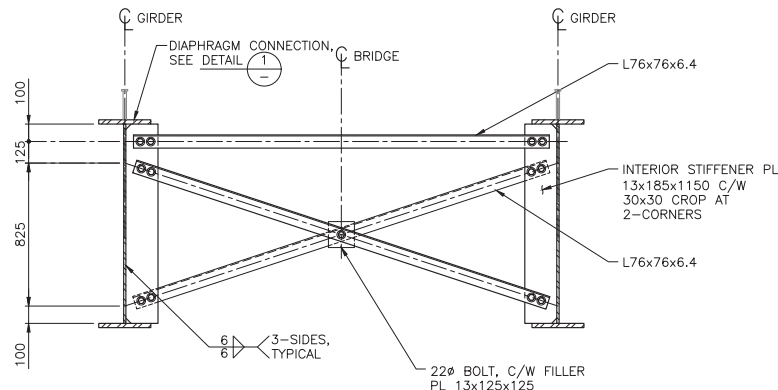
FIXED END OF BRIDGE

EXPANSION END OF BRIDGE



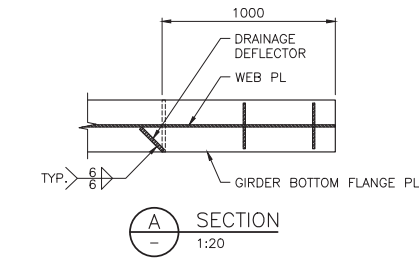
SECTION - DIAPHRAGM AT END

1:20
NOTE: 2-REQUIRED.



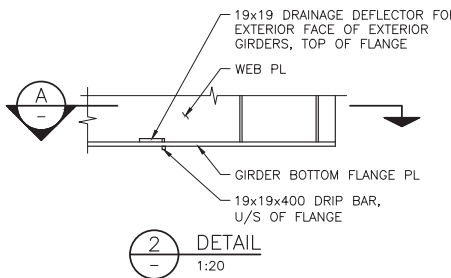
SECTION - INTERMEDIATE DIAPHRAGM

1:20
NOTE: 7-REQUIRED.



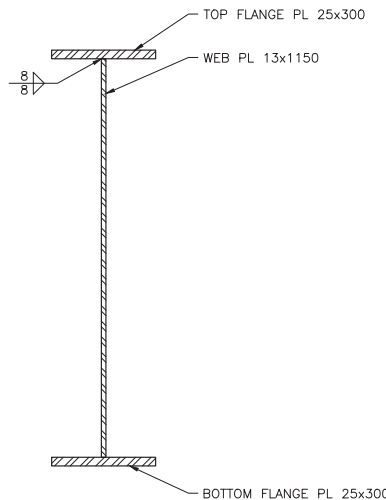
SECTION

1:20



DETAIL

1:20



SECTION - GIRDER

1:10

ISSUED FOR REVIEW

NOT FOR
CONSTRUCTION

NOTES:

- FOR GENERAL NOTES, SEE DWG. S01 AND S02.
- ALL STEELWORK NEXT TO ABUTMENTS AS INDICATED ON DRAWINGS TO BE PAINTED PER SPECIFICATIONS EXCEPT TOP FLANGE OF DIAPHRAGM, TOP FLANGE OF GIRDERS AND CONNECTION FAYING SURFACES. COLOUR OF PAINT TO MATCH WEATHERED STEEL SURFACE.

ISSUES				SUB CONSULTANT			
No.	DATE	ISSUED FOR		No.	DATE	ISSUED FOR	
A	2018.11.09	CLIENT REVIEW					
B	2019.01.11	CLIENT REVIEW					

DRAFTED
PHU

DRAFTING REVIEW

DESIGNED
SJS

DESIGN REVIEW

HEROLD ENGINEERING

3701 Shenton Rd., Nanaimo, BC V9T 2H1
Tel: 250-751-8558 Fax: 250-751-8559
Email: mail@heroldengineering.com

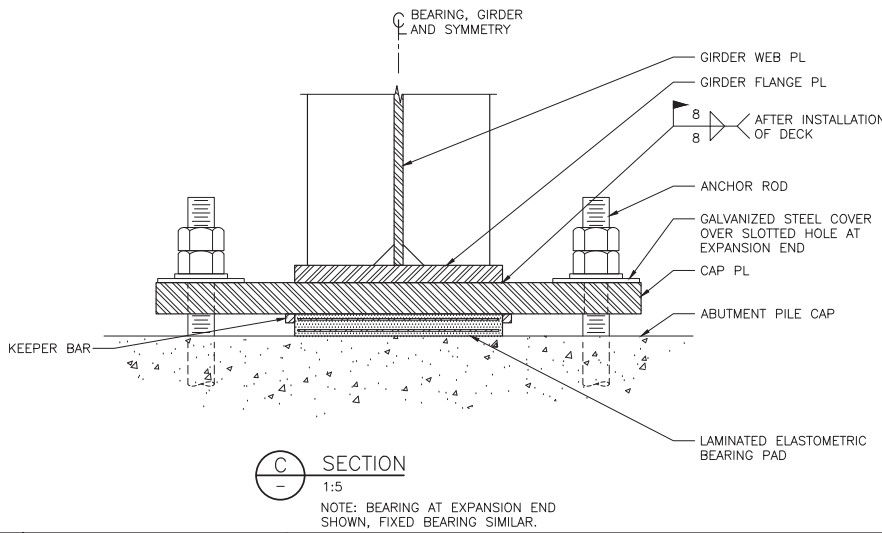
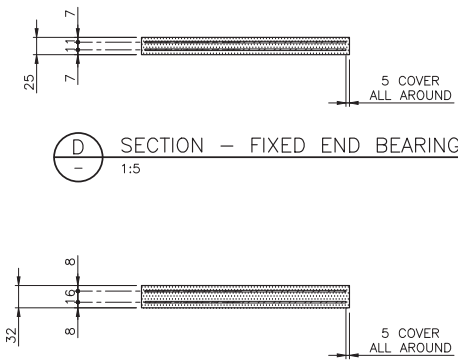
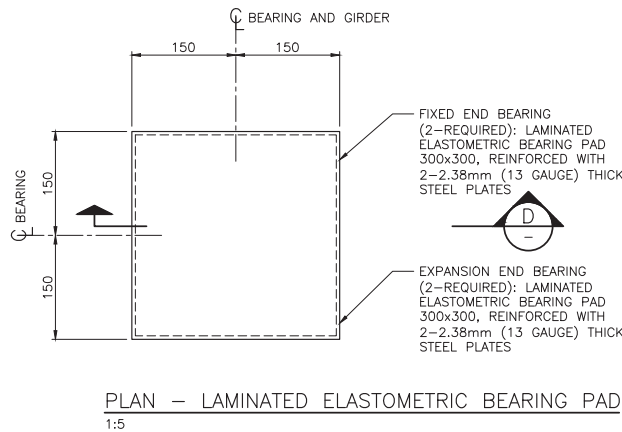
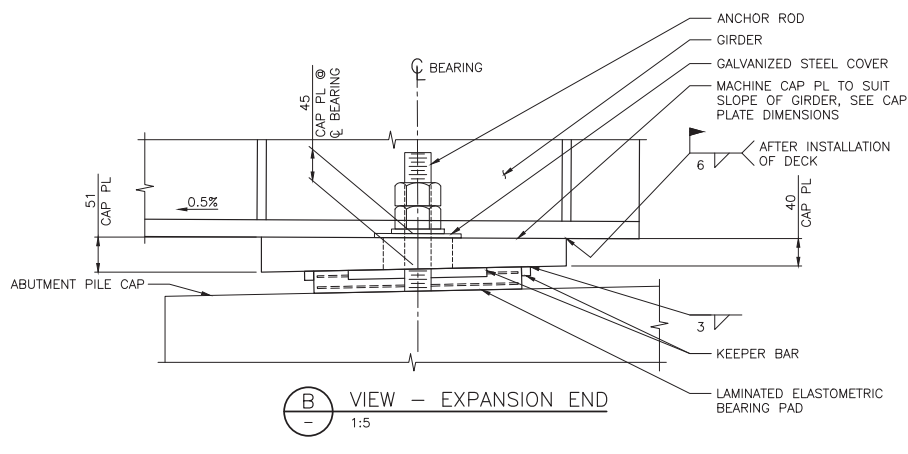
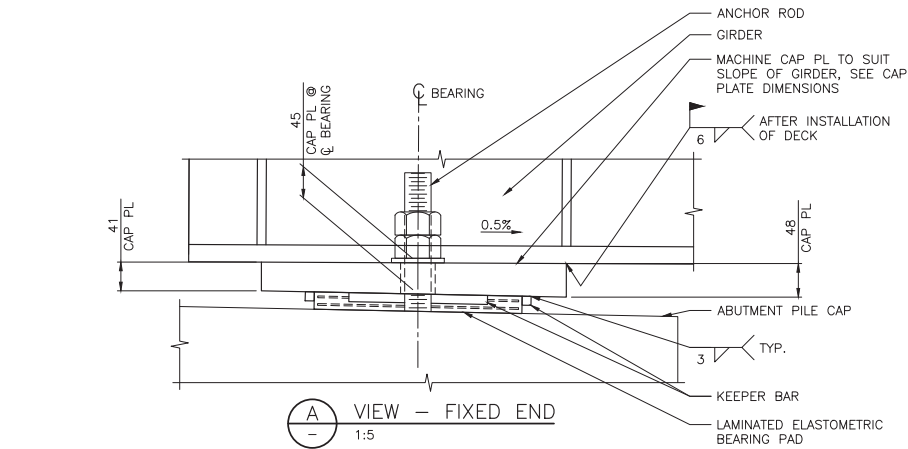
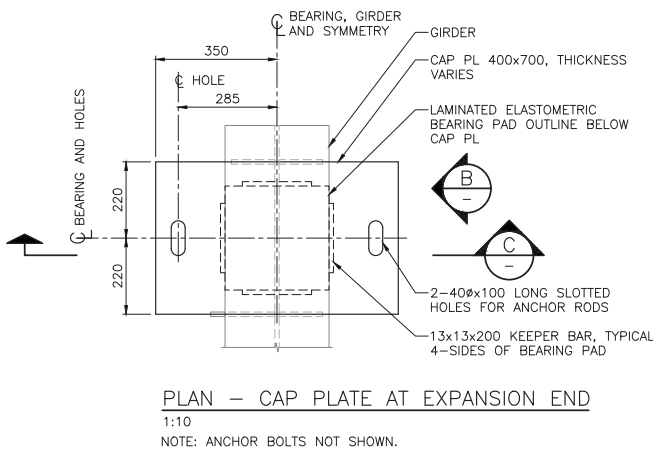
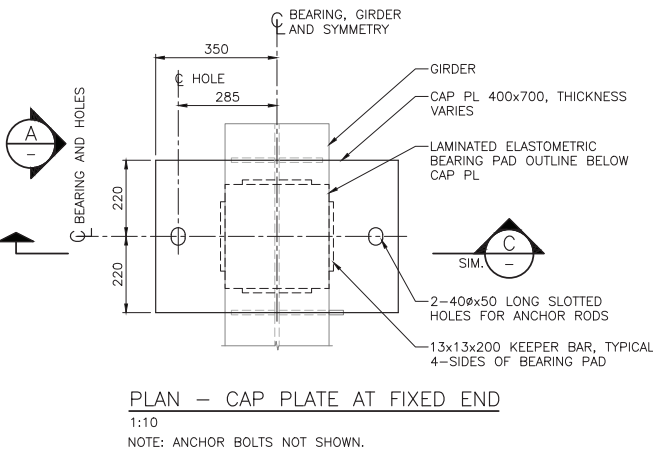
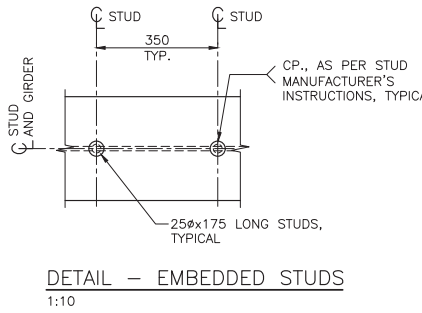
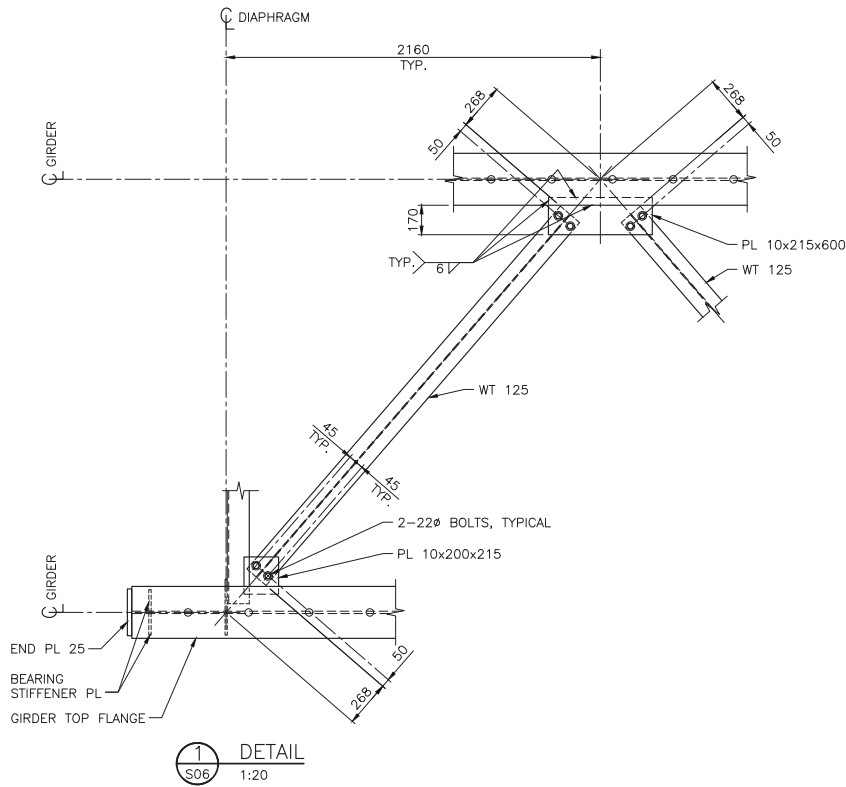
ENGINEERS SEAL

STEEL GIRDERS
- SHEET 1

LITTLE QUALICUM RIVER
PEDESTRIAN BRIDGE
6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2
REGIONAL DISTRICT OF NANAIMO

HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A
SCALE AS SHOWN	PERMIT No. N/A
HEL DRAWING No. S07	REVISION B

DESTROY ALL DRAWINGS SHOWING PREVIOUS REVISION



ISSUED FOR REVIEW

NOT FOR CONSTRUCTION

NOTES:
1. FOR GENERAL NOTES, SEE DWG. S01 AND S02.

ISSUES							
No.	DATE	mm/yy	ISSUED FOR	No.	DATE	mm/yy	ISSUED FOR
A	2018.11.09		CLIENT REVIEW				
B	2019.01.11		CLIENT REVIEW				

The Copyright reserved. This drawing remains the exclusive property of Harold Engineering Limited and may not be reused or reproduced without written consent of Harold Engineering Limited.

SUB CONSULTANT	

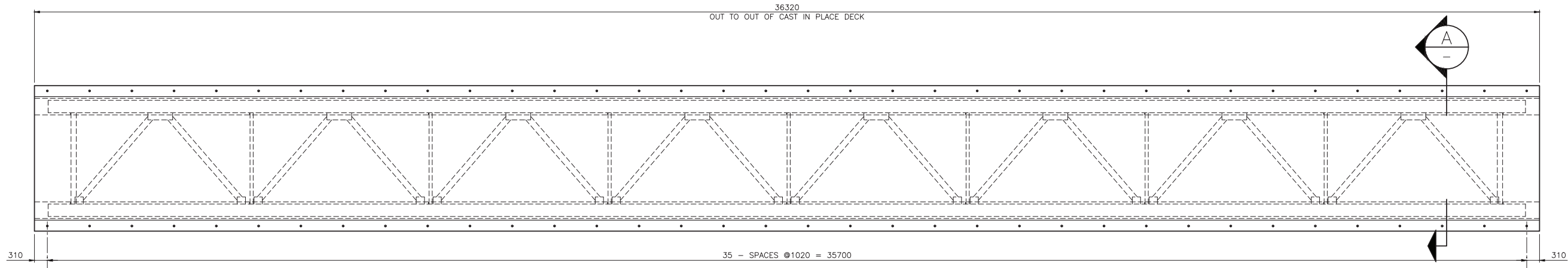
DRAFTED PHU	 3701 Shenton Rd, Nanaimo, BC V9T 2H1 Tel: 250-751-8558 Fax: 250-751-8559 Email: mail@heroldengineering.com
DRAFTING REVIEW	
DESIGNED SJS	
DESIGN REVIEW	

ENGINEERS SEAL	

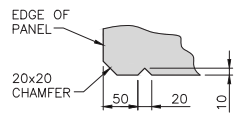
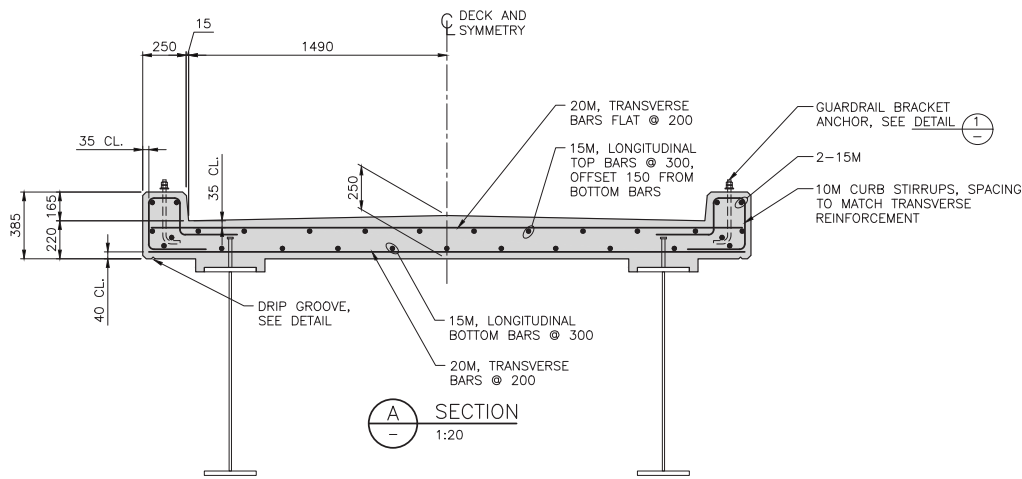
STEEL GIRDERS - SHEET 2	

LITTLE QUALICUM RIVER PEDESTRIAN BRIDGE 6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2 REGIONAL DISTRICT OF NANAIMO	

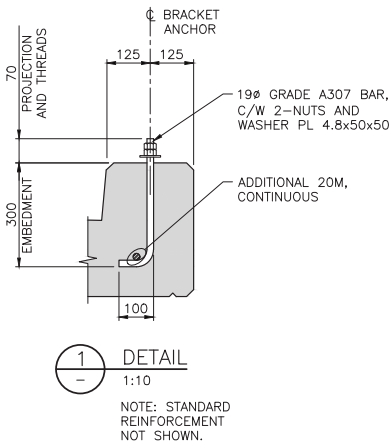
HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A
SCALE AS SHOWN	PERMIT No. N/A
HEL DRAWING No. S08	REVISION B



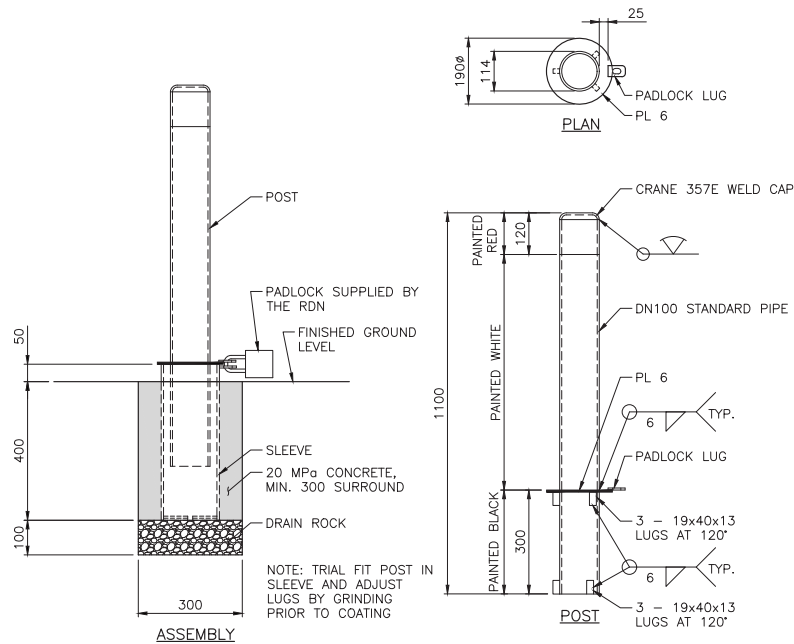
PLAN - CAST-IN-PLACE DECK GENERAL ARRANGEMENT
1:50



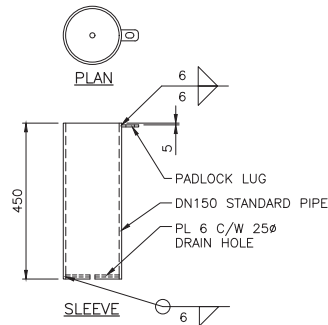
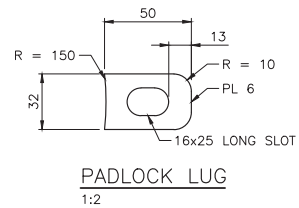
DETAIL - DRIP GROOVE
1:5
NOTE: REINFORCEMENT NOT SHOWN.



NOTE: STANDARD REINFORCEMENT NOT SHOWN.



NOTE: 2 - REQUIRED, 1 AT EACH END OF BRIDGE



ISSUED FOR REVIEW

NOT FOR
CONSTRUCTION

NOTES:

1. FOR GENERAL NOTES, SEE DWG. S01 AND S02.

ISSUES						SUB CONSULTANT
No.	DATE	ISSUED FOR	No.	DATE	ISSUED FOR	
A	2018.11.09	CLIENT REVIEW				
B	2019.01.11	CLIENT REVIEW				

DRAFTED PHU
DRAFTING REVIEW
DESIGNED SJS
DESIGN REVIEW

HEROLD ENGINEERING

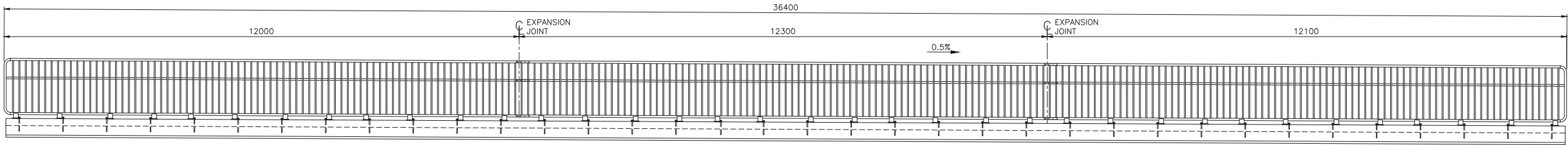
3701 Shenton Rd., Nanaimo, BC V9T 2H1
Tel: 250-751-8558 Fax: 250-751-8559
Email: mail@heroldengineering.com

ENGINEERS SEAL

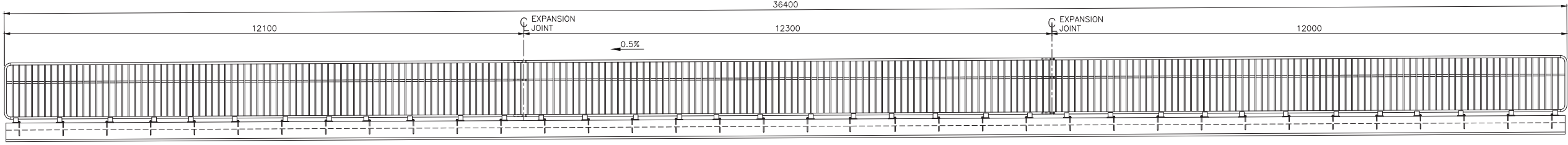
CONCRETE BRIDGE
DECK AND
REMOVABLE
BOLLARD

LITTLE QUALICUM RIVER
PEDESTRIAN BRIDGE
6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2
REGIONAL DISTRICT OF NANAIMO

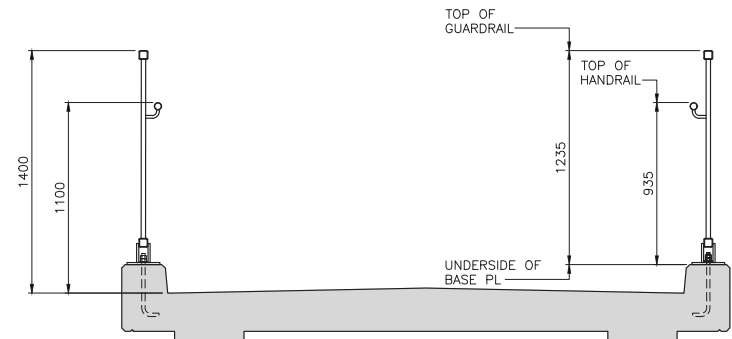
HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A
SCALE AS SHOWN	PERMIT No. N/A
HEL DRAWING No. S09	REVISION B



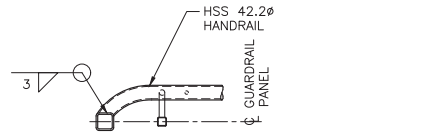
ELEVATION LOOKING EAST
1:50



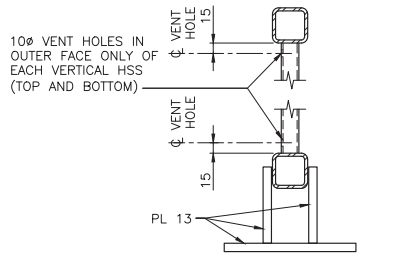
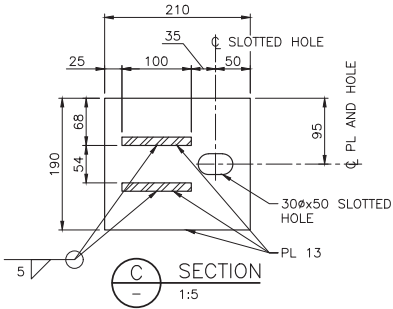
ELEVATION LOOKING WEST
1:50



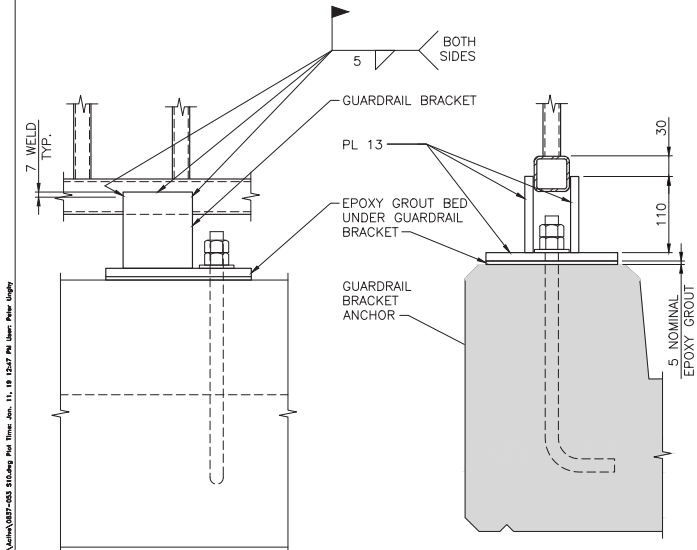
TYPICAL SECTION - BRIDGE DECK
1:20



SECTION - HANDRAIL TERMINATION
1:10
NOTE: HANDRAIL TERMINATION AT END OF PANEL IS TYPICAL FOR ALL PANELS.



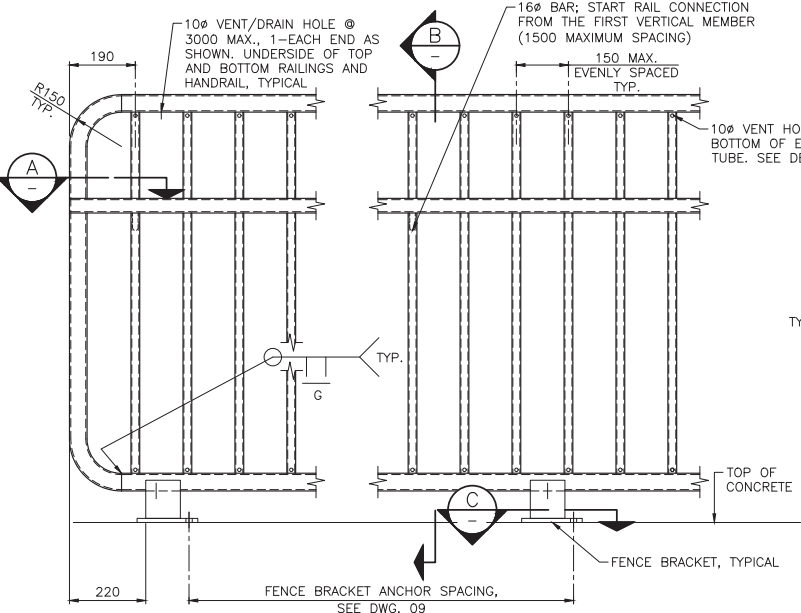
DETAIL - VENT HOLE
1:5



ELEVATION
1:5

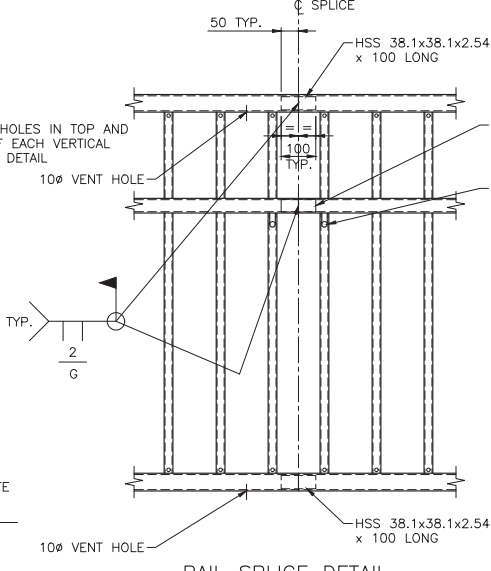
SECTION
1:5

DETAIL - GUARDRAIL BRACKET
1:5

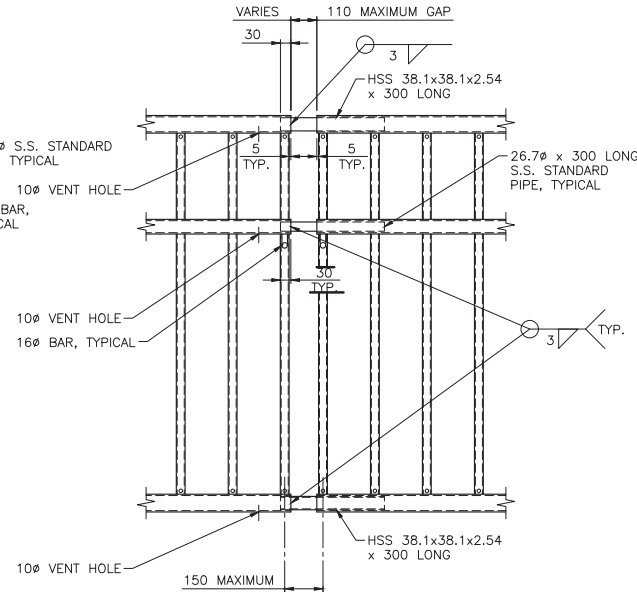


END DETAIL
1:10

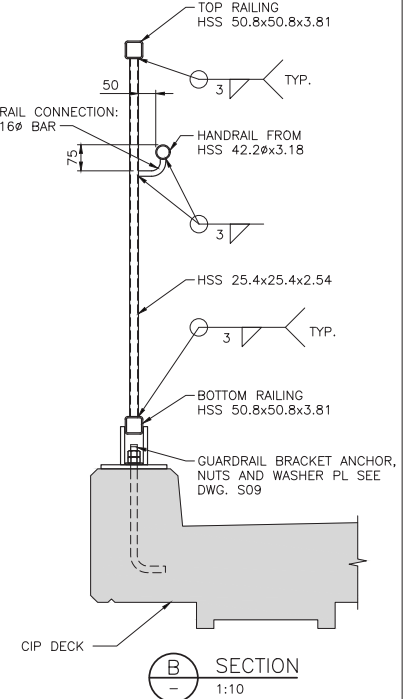
TYPICAL DETAIL
1:10



RAIL SPLICE DETAIL
1:10



RAIL EXPANSION JOINT
1:10
NOTE: RAILING EXPANSION JOINT TO BE LOCATED AT 30 METRES MAXIMUM SPACING.



- NOTES:
- FOR GENERAL NOTES, SEE DWG. S01 AND S02.
 - GUARDRAILS SHALL BE COATED WITH A DUPLEX SYSTEM CONSISTING OF MANUFACTURER'S STANDARD BLACK POWDER COATING OVER HOT DIP GALVANIZING AS FOLLOWS:
 - HOT DIP GALVANIZE IN ACCORDANCE WITH ASTM A123 AND DO NOT WATER OR CHROMATE QUENCH.
 - REMOVE ALL DRAINAGE SPIKES AND SURFACE DEFECTS.
 - POWDER COATING CONTRACTOR IS RESPONSIBLE FOR INSPECTING THE GALVANIZED MATERIAL AT THE GALVANIZING SHOP PRIOR TO SHIPMENT, BASED ON REQUIREMENTS GIVEN IN ASTM A123.
 - PREPARE GALVANIZED PIECES FOR POWDER COATING IN ACCORDANCE WITH ASTM D7803.
 - POWDER-COAT WITHIN 48 HOURS OF GALVANIZING. DO NOT GET SURFACES WET. DO NOT LEAVE OUTSIDE.
 - KEEP THE SURFACE CLEAN. DO NOT TRANSPORT UNCOVERED LOADS. DIESEL FUMES WILL CONTAMINATE SURFACE.
 - IF SURFACE CONTAMINATION HAS OCCURRED, CLEAN SURFACE WITH PROPRIETARY SOLVENT/DETERGENT DESIGNED FOR PRE-CLEANING, PRIOR TO POWDER-COATING.
 - USE SWEEP BLASTING OR ZINC PHOSPHATE PRE-TREATMENT.
 - PRE-HEAT WORK PRIOR TO POWDER APPLICATION.
 - USE DEGASSING GRADE POLYESTER POWDER ONLY.
 - CHECK FOR CORRECT CURING BY SOLVENT TESTING. ADJUST PRE-HEAT AND LINE SPEED TO ENSURE FULL CURE.
 - PROVIDE 10 YEAR RUST THROUGH WARRANTEE. INCLUDE TERMS AND CONDITIONS OF WARRANTEE IN BID.

ISSUED FOR REVIEW

NOT FOR
CONSTRUCTION

ISSUES			
No.	DATE	ISSUED FOR	
A	2019.01.11	CLIENT REVIEW	

SUB CONSULTANT	

DRAFTED PHU	HEROLD ENGINEERING 3701 Shenton Rd., Nanaimo, BC V9T 2H1 Tel: 250-751-8558 Fax: 250-751-8559 Email: mail@heroldengineering.com
DRAFTING REVIEW	
DESIGNED SJS	
DESIGN REVIEW	

ENGINEERS SEAL

GUARDRAILS

LITTLE QUALICUM RIVER PEDESTRIAN BRIDGE 6300 HAMMOND BAY ROAD NANAIMO BC V9T 6N2 REGIONAL DISTRICT OF NANAIMO

HEL PROJECT No. 0837-053	CLIENT DWG. No. N/A
SCALE AS SHOWN	PERMIT No. N/A
HEL DRAWING No. S10	REVISION A

LQRRP Bridge Replacement Project
Detailed Design and Class B Costing

14-Jan-18

SCHEDULE OF QUANTITIES AND ESTIMATED PRICES

SECTION	DESCRIPTION	ITEM COST
1	GENERAL	\$48,500.00
	<ul style="list-style-type: none"> Includes mobilization, quality and traffic management, detour and detour bridge, site office, and site modifications. 	
2	BRIDGE CONSTRUCTION	\$471,590.00
	<ul style="list-style-type: none"> Foundation excavation and backfill Steel piles Formwork Reinforcing steel Concrete Steel girders and braces 	
3	ROADWORKS AND APPROACHES	\$21,200.00
	<ul style="list-style-type: none"> Clearing and grubbing Roadway and drainage Granular materials Access road repair 	
4	SIGNING	\$8,650.00
	<ul style="list-style-type: none"> Supply and install removeable, locking bollards Supply and install new signs 	
SUBTOTAL		\$549,940.00
ADD Engineering and Project Manager Services at 15%		\$82,491.00
ADD General Contractor - Admin and General Requirements of 15%		\$82,491.00
ADD Licensing and Permitting Costs		\$15,000.00
ADD Construction Contingency of 20%		\$109,988.00
TOTAL		\$839,910.00

TO: Regional Parks and Trails Select Committee

MEETING: February 5, 2019

SUBJECT: Report Review - RDN Parks Funding Service Review

At the January 08, 2019 COW meeting the following resolution was carried:

It was moved and seconded that a review of the existing funding mechanism for Regional Parks be incorporated into the new Regional Parks and Trails Master Plan development.

It was moved and seconded that the following motion be referred to the Regional Parks and Trails Select Committee:

That a review of the existing funding mechanism for Regional Parks be incorporated into the new Regional Parks and Trails Master Plan development.

Attached is the RDN Parks Funding Service Review Report that was received at the December 12th, 2017 Board meeting.



RECOMMENDATIONS

1. That the RDN Parks and Trails Funding Service Review conducted by Neilson-Welch Consulting be received.
2. That the RDN Parks and Trail Funding Service Review recommendations be implemented for 2018/2019 work plans and the 2019 to 2023 Financial Plan.
3. That the existing Regional Parks Parcel tax be utilized for the 2018 budget year.

With the RDN Parks Funding Service Review now complete, the report's findings and recommendations are ready for the Regional Board's review and consideration.

During these deliberations by the Board, additional information was requested on other forms of taxation that the Board could use as an alternative to the parcel tax approach for this service area. Staff conducted the review and provided a report titled *Regional Parks Parcel Tax Review (September 8, 2016)* which outlined various forms of taxation alternatives for the Regional Parks Acquisition and Capital Development Fund. Upon receiving the report, the Board opted to maintain the status quo and continue using a parcel tax at \$14.00 until a Regional Services Review was completed.

At the Regional Board meeting held January 24, 2017 staff were directed to review the Regional Parks and Trails service funding allocations and bring back a report on options for funding the service in the future.

As Regional Parks share staff and administrative resources with Electoral Area Community Parks and have similar financial tools that can be used for acquisition and capital development, the consulting team was also requested to consider full RDN parks system in their analysis.

Neilson-Welch Consulting was then retained to undertake the service review per the following scope of work:

1. Review current funding models in use at the RDN for Regional and Community Parks.
2. Research funding models and financial tools in use at other Regional Districts and local governments.
3. Examine the acquisition, development, management and use of Regional and Community Parks and Trails to ensure equity in the service is being met across the RDN's member participants.
4. Based on the review of items 1, 2 and 3 above, propose funding options and recommendations for the RDN Regional Board's consideration.

Neilson-Welch Consulting has concluded the review and their report, provided in Attachment 2, is ready for the Regional Board's review and consideration.

As part of the review, the consultants concluded that the level of equity across participating jurisdictions and among individual taxpayers in the Regional Park Service would be improved by allocating acquisition, capital development and operating costs using a combination (50%-50%) of converted assessment and population for Regional Parks.

In addition, equity among individual taxpayers would be further improved through the introduction of Development Costs Charges (DCCs) to assist in funding land acquisitions and development for Regional Parks.

The report also recommends the RDN continue to separate acquisition funding from operating funding, irrespective of the approaches taken to cost allocation and taxation. It is also recommended that the RDN confirm that the Regional Park Acquisition and Capital Development Reserve Fund has the flexibility required to allow for spending on development projects.

For Electoral Area Community Parks, the Service Review recommends no significant changes in the way funds are collected and used. The report does recommend implementing DCCs for park improvements in the eight service areas.

ALTERNATIVES

1. That the RDN Parks and Trails Funding Service Review conducted by Neilson-Welch Consulting be received, the report's recommendations be added to the 2018/2019 work plans and 2019 to 2023 Financial Plan for implementation, and the existing Regional Parks Parcel Tax be utilized for the 2018 budget year.
2. That the RDN Parks and Trails Funding Service Review conducted by Neilson-Welch Consulting be received, the report's recommendations be considered further by the Board prior to the review of the 2019 Budget and Five Year Financial Plan, and the existing Regional Parks Parcel Tax be utilized for the 2018 budget year.
3. That the RDN Parks and Trails Funding Service Review conducted by Neilson-Welch Consulting be received and alternate Board direction be provided on the funding of RDN regional and community parks.

FINANCIAL IMPLICATIONS

One of the primary recommendations the have been brought forward in the Service Review is to allocate acquisition, capital development and operating costs using a combination (50%-50%) of converted assessment and population for Regional Parks. The existing model allocates acquisition and capital costs based on number of parcels, and operating costs based on population. Figure I.3.2 from the Service Review report and shown below, highlights the financial implications for this change to each participant using the current value collected in 2017 by the 14.00 parcel tax for acquisition and capital costs and by way of population for operational cost.

Figure I.3.2

Impact of Allocating All Costs by Converted Assessment and Population (50-50)

Jurisdiction	Existing Model			Converted Assess & Population (50-50)			Change in Allocation
	Acquisit/Dev	Ops	Total	Acquisit/Dev	Ops	Total	
City of Nanaimo	481,166	797,168	1,278,334	536,770	765,873	1,302,643	24,309
City of Parksville	94,318	110,225	204,543	78,963	112,666	191,630	(12,913)
Town of Qualicum	67,774	78,771	146,545	60,171	85,854	146,025	(520)
District of Lantzville	20,748	31,753	52,501	22,941	32,732	55,673	3,172
Electoral Area A	40,628	62,168	102,796	39,278	56,042	95,320	(7,476)
Electoral Area B	52,794	35,523	88,317	28,822	41,124	69,945	(18,372)
Electoral Area C	19,950	24,733	44,683	23,299	33,243	56,542	11,859
Electoral Area E	47,922	53,950	101,872	47,103	67,208	114,311	12,439
Electoral Area F	41,916	68,034	109,950	42,767	61,020	103,787	(6,163)
Electoral Area G	52,206	65,752	117,958	47,741	68,118	115,859	(2,099)
Electoral Area H	35,350	34,210	69,560	26,918	38,406	65,324	(4,236)
Total Requisition	954,772	1,362,287	2,317,059	954,772	1,362,287	2,317,059	-

As it will take additional time that will extend past the 2018 budget approval cycle to work with participating jurisdictions to amend *Bylaw No. 1231 (2001)*, it is recommended that the acquisition and capital parcel tax and the population allocation of the requisition for operations be maintained for the 2018 budget year.

The report also concluded that Development Cost Charges (DCC's) should be a tool to use in order to meet the future demand for both Regional Parks and Electoral Area Community Parks. When last reviewed in 2011 by the RDN, it was estimated that implementing DCC's could collect in the range of 19 to 24 million dollars over a 30 year period to assist with acquisitions and the development of the Regional Parks system. The DCC program would need to be reviewed in relation to updated acquisition and capital development plans; however, a DCC program could be expected to provide significant funding toward Regional Parks acquisitions and development.

STRATEGIC PLAN IMPLICATIONS

Undertaking the service review on funding for Regional Parks and Trails and Electoral Area Community Parks is in alignment with the RDN Boards Strategic Plan in the following strategic priority areas:

Service and Organization Excellence

- Review the costs and benefits during the investment of regional services.
- Advocate for Active Transportation which includes use of the RDN trail system.
- Recognize recreational services as a core service.

Focus on Relationships

- Look at opportunities to partner with other branches of government and community groups to advance the Regional District of Nanaimo.

Focus Economic Health

- Recognize eco-tourism as a key opportunity in the region.

Focus on the Environment

- Protecting and enhancing our environment in all decisions.



Tom Osborne
tosborne@rdn.bc.ca
November 22, 2017

Reviewed by:

- C. Midgley, Manager of Strategic Initiatives and Asset Management
- W. Idema, Director of Finance
- G. Garbutt, Acting Chief Administrative Officer

Attachments

1. Summary of Recommendations from RDN Regional Parks Funding Service Review
2. Regional Parks and Trails Funding Service Review / Neilson-Welch Consulting

Attachment 1

**Summary of Recommendations from
RDN Regional Parks Funding Service Review**

Topic	Recommendations
Regional Service Funding Model	<p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to allocate land acquisition and development costs among service participants on the combination (50-50) of converted assessment and population, rather than number of parcels.</p> <p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to replace the property parcel tax for acquisition and development costs with a property value tax.</p> <p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to allocate service operating costs among service participants on the combination (50-50) of converted assessment and population, rather than population alone.</p> <p>THAT the Board direct staff to undertake a survey of regional parks and trails users, at key times of year, every three-to-five years, to identify and track the home jurisdictions of users.</p> <p>THAT the Board, pursuant to section 559(2) of the <i>Local Government Act</i>, introduce a Development Cost Charge to assist in raising funds required for parkland acquisition, and parkland improvements.</p> <p>THAT the Board direct staff to review the existing permit fees charged for special events, filming, and commercial activities, and to propose a new revenue-generating fee schedule.</p> <p>THAT the Board continue its approach of collecting land acquisition and capital development funds separately from funds that are collected to support planning, operations and maintenance.</p> <p>THAT the Board clarify in all materials that monies held in the Regional Parks Acquisition and Capital Development (Reserve) Fund are intended both for land acquisition and capital project purposes.</p>

Topic	Recommendations
Community Services Funding Model	<p>THAT the Board retain its current practice of allocating staffing costs equally across the Electoral Areas.</p> <p>THAT the Board continue to raise service funds using property value taxes.</p> <p>THAT the Board, pursuant to section 559(2) of the <i>Local Government Act</i>, introduce local Development Cost Charges to assist in raising funds required for parkland improvements.</p>
Additional Issues	<p>THAT the Board refrain from assuming responsibility, in whole or part, for municipal parks that may possess regional park characteristics.</p> <p>THAT the Board direct staff to work with their counterparts in the Regional District's member municipalities on developing and implementing an integrated planning framework for regional and local parks and trails.</p>



RDN PARKS FUNDING SERVICE REVIEW REPORT

This *Report* has been prepared by Neilson-Welch Consulting Inc. for the Regional District of Nanaimo (RDN). The document is presented for discussion with, and for the sole use of, the RDN. No representations of any kind are made by the consultants to any party with whom the consultants do not have a contract.

NEILSON-WELCH
CONSULTANTS TO GOVERNMENT

Neilson-Welch Consulting Inc.
1-600 Sherwood Road
Kelowna, BC, V1W 5K1
aneilson@nwci.ca

November, 2017

TABLE OF CONTENTS

EXECUTIVE SUMMARY	iii
INTRODUCTION	1
Approach to Work.....	1
Format of Report	2
Part I: Regional Parks and Trails Service	2
Part II: Community Parks and Trails Services	3
PART I: REGIONAL PARKS AND TRAILS SERVICE.....	4
I.1 CURRENT SERVICE	5
Overview of Regional Parks & Trails Service	5
Funding Model	6
Land Acquisition	6
Parks and trails Development	8
Planning, Operations and Maintenance.....	9
Challenges Related to Funding Model	9
I.2 FINANCIAL TOOLS	12
Comparative Research	12
Financial Tools	12
Land Acquisition	13
Parks and trails Development	17
Planning, Operations and Maintenance.....	18
I.3 ASSESSMENT OF FUNDING MODEL.....	19
Evaluation Criteria	19
Regional Parks & Trails Funding Model.....	19
Assessment of Regional Service Funding Model	20
Equity (Jurisdictions)	20
Equity (Individual Taxpayers)	24
Effectiveness	25
Transparency.....	27
Recommendations on Regional Service Funding Model	27



I.4 ADDITIONAL ISSUES	29
Municipal Parks	29
Integrated Planning	31
Recommendations	31
I.5 SUMMARY OF RECOMMENDATIONS.....	32
PART II: COMMUNITY PARKS AND TRAILS SERVICES	34
II.1 CURRENT SERVICES	35
Funding Model	35
Challenges Related to Funding Model	37
II.2 FINANCIAL TOOLS.....	38
Community Parks & Trails Services	38
Land Acquisition	38
Parks and trails Development	39
Planning, Operations and Maintenance.....	40
II.3 ASSESSMENT OF FUNDING MODEL.....	41
Community Parks & Trails Funding Model	41
Assessment of Community Services Funding Model.....	41
Equity (Jurisdictions)	41
Equity (Individual Taxpayers)	43
Effectiveness	43
Transparency.....	44
Recommendations on Community Services Funding Model.....	44
II.4 SUMMARY OF RECOMMENDATIONS.....	45

**RDN
PARKS FUNDING
SERVICE REVIEW
REPORT**

NEILSON-WELCH
CONSULTANTS TO GOVERNMENT

EXECUTIVE SUMMARY

Neilson-Welch Consulting Inc. was retained by the Regional District of Nanaimo (RDN) to undertake the *RDN Parks Funding Service Review*. The purpose of the *Service Review* is to assess and make recommendations on the funding model that is currently used to support the acquisition, development and operation of parks and trails in the RDN's Regional Parks & Trails Service. The assessment of the funding model considers a variety of criteria, the most important of which is equity among participating jurisdictions in the service.

While the funding model of the regional service was identified in the *Review's* terms of reference as the primary focus of the assignment, the consultants were also asked to assess and make recommendations on the funding model in place for the Community Parks & Trails Services in the RDN's seven electoral areas.

THE REPORT

This report presents the results of the *Service Review*. The document is divided into two parts.

Part I: Regional Parks and Trails Service

The first and largest part of the report focuses on the Regional Parks and Trails Service. Part I is divided into five chapters:

- *Chapter I.1: Current Service* — Chapter I.1 profiles the RDN's Regional Parks & Trails Service. The profile highlights the funding model and financial tools in place today, and the changes that have occurred to service funding since the service's inception. The profile also identifies challenges facing the service.
- *Chapter I.2: Service Funding* — Chapter I.2 outlines the full range of financial tools available to regional districts to assist in funding the acquisition, development and operation of regional parks and trails. The text draws heavily on comparative research undertaken on regional parks and trails services across British Columbia.
- *Chapter I.3: Assessment of Service Funding Models* — Chapter I.3 introduces and applies a set of evaluation criteria for assessing the RDN's Regional Parks & Trails funding model. Included in the list of criteria are:
 - equity across jurisdictions
 - equity among different types of taxpayers
 - effectiveness at raising sufficient revenue for the service
 - transparency in communicating the purposes of monies raised

- *Chapter I.4: Additional Issues* — This chapter examines specific issues that arose over the course of the *Review*, but that do not necessarily fit into the discussions on funding models.
- *Chapter I.5: Summary of Recommendations* — The final chapter summarizes the recommendations on the regional service funding model, and on the issues raised in Chapter I.4.

Part II: Community Parks and Trails Services

The second part of the report examines the funding model in place for the eight Community Parks and Trails Services. Chapter II.1 begins by profiling the services and their funding model. Chapter II.2 then considers the range of financial tools available to regional districts to assist in the acquisition, development and operation of parks and trails at the community level. The current funding model is assessed in Chapter II.3 using the same criteria introduced for the regional service. Recommendations are summarized in Chapter II.4.

FINDINGS

Regional Parks and Trails Service

A number of key findings emerged from the assessment of the funding model for the Regional Parks and Trails Service:

- *Equity (Jurisdictions)* — The level of equity across participating jurisdictions would be improved if all service costs — acquisition, development, operating — were allocated on a combination (50-50) of converted assessment and population. This approach would recognize the service's indirect benefits to the broader region, but also the service's direct benefits to residents in each jurisdiction. Under the current model, costs for acquisition and development are allocated on basis of parcels; operating costs are allocated by population alone.
- *Equity (Individual Taxpayers)* — Equity among individual taxpayers would be improved through the use of a property value tax in place of the current property parcel tax to determine and collect service payments from properties. Equity among individual taxpayers would also be improved through the introduction of a development cost charge (DCC) to assist in funding land acquisitions and development.
- *Effectiveness* — The current funding model, with its reliance on property tax revenues only, does not appear to provide sufficient funding to meet the expressed expectations and interests related to the service. The RDN should consider introducing a regional parks and trails DCC to increase and diversify funding. User fees for special events, filming and other permits should also be reviewed and increased where warranted. The Regional District may also need to increase the overall amount it collects in service tax revenues to support the levels of service expected by residents.

- *Transparency* — Support for regional services increases when funding models are transparent in their use of tax dollars — that is, when funds raised are used in accordance with their stated purpose. At the RDN, transparency in the Regional Parks & Trails Service funding model is enhanced by the separation of acquisition and development funds from operational funds. This separation should continue, irrespective of the approaches taken to cost allocation and taxation. The RDN should also ensure that monies held in its Regional Park Acquisition and Capital Development Fund are identified consistently as funds that are intended for both acquisition and development purposes.

Additional Issues Considered

As noted earlier, over the course of the *Service Review* certain additional issues arose that should be considered, but that do not fit neatly into the discussions on funding models. The first issue concerns the potential for the RDN to assume responsibility, in whole or part, for municipal parks that have regional park characteristics. There are many examples of municipalities in the province that provide region-like parks. Several regional districts have been faced with the prospect of assuming responsibility for these parks; in general, regional districts have been reluctant to accept any responsibility.

The second issue concerns the potential for an integrated approach to parks and trails planning that would take into account municipal and electoral area park systems, along with the regional parks and trails system. There is considerable interest on the part of the RDN and member municipalities to integrate their respective efforts. The upcoming process for updating the *Regional Parks & Trails Plan* provides an opportunity to work together.

Community Parks and Trails Services

The funding model for the Community Parks & Trails Services was assessed using the same evaluation criteria that were introduced for the regional service. Key findings are as follows:

- *Equity (Jurisdictions)* — Inter-jurisdictional equity considerations at the local service level relate to the allocation, across local service areas, of the cost of Parks and Recreation staff who are assigned to services. At the RDN, this cost is allocated in equal portions to the seven electoral areas. This approach may, at first glance, seem unfair given differences between and among the local services. The approach, however, can be supported by a number of points, as explained in the report, and should be maintained.
- *Equity (Individual Taxpayers)* — The RDN should leave unchanged its reliance on property value taxes for the local services — this method of taxation is most equitable for the services. Overall equity would be improved,

however, if RDN introduced a DCC specifically for community parks and trails improvements.

- *Effectiveness* — A new DCC, focused initially on improvements, should be considered. In the future, the tool could be expanded to include land acquisition in the event that the Regional District begins to fund acquisition efforts directly with tax dollars.
- *Transparency* — No issues were identified concerning transparency.

RECOMMENDATIONS

Figure ES.1 presents the consultants' recommendations, for the Board's consideration, from both parts of the report. Included are recommendations on the funding model for the Regional Parks and Trails Service, the additional issues considered, and the funding model for the Community Parks and Trails Services.

Figure ES.1
Recommendations

Topic	Recommendations
Regional Service Funding Model	<p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to allocate land acquisition and development costs among service participants on the combination (50-50) of converted assessment and population, rather than number of parcels.</p> <p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to replace the property parcel tax for acquisition and development costs with a property value tax.</p> <p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to allocate service operating costs among service participants on the combination (50-50) of converted assessment and population, rather than population alone.</p> <p>THAT the Board direct staff to undertake a survey of regional parks and trails users, at key times of year, every five (5) years, to identify and track the home jurisdictions of users.</p> <p>THAT the Board, pursuant to section 559(2) of the <i>Local Government Act</i>, introduce a Development Cost Charge to assist in raising funds required for parkland acquisition, and parkland improvements.</p>

Topic	Recommendations
	<p>THAT the Board direct staff to review the existing permit fees charged for special events, filming, and commercial activities, and to propose a new revenue-generating fee schedule.</p> <p>THAT the Board continue its approach of collecting land acquisition and capital development funds separately from funds that are collected to support planning, operations and maintenance.</p> <p>THAT the Board clarify in all materials that monies held in the Regional Parks Acquisition and Capital Development (Reserve) Fund are intended both for land acquisition and capital project purposes.</p>
Additional Issues	<p>THAT the Board refrain from assuming responsibility, in whole or part, for municipal parks that may possess regional park characteristics.</p> <p>THAT the Board direct staff to work with their counterparts in the Regional District's member municipalities on developing and implementing an integrated planning framework for regional and local parks and trails.</p>
Community Services Funding Model	<p>THAT the Board retain its current practice of allocating staffing costs equally across the Electoral Areas.</p> <p>THAT the Board continue to raise service funds using property value taxes.</p> <p>THAT the Board, pursuant to section 559(2) of the <i>Local Government Act</i>, introduce local Development Cost Charges to assist in raising funds required for parkland improvements.</p>

INTRODUCTION

Neilson-Welch Consulting Inc. was retained by the Regional District of Nanaimo (RDN) to undertake the *RDN Parks Funding Service Review*. The purpose of the *Service Review* is to assess and make recommendations on the funding model that is currently used to support the acquisition, development and operation of parks and trails in the RDN's Regional Parks & Trails Service. The assessment of the model considers a variety of criteria, the most important of which is equity among participating jurisdictions in the service.

The funding model of the regional service was identified in the *Review's* terms of reference as the primary focus of the assignment. The consultants were also asked, however, to assess and make recommendations on the funding model in place for the Community Parks & Trails Services in the Regional District's seven electoral areas.

The findings and recommendations from the *Review* are intended to help the RDN in its efforts to fund parks and trails services in ways that support the purposes of the services, and that are fair to taxpayers throughout the region. The recommendations will also help to inform the development of an updated *Regional Parks & Trails Plan* in 2018.

APPROACH TO WORK

The consultants' approach to the *Service Review* consisted of the following elements:

- *Background Research* — The consultants reviewed a considerable number of documents concerning the regional and community services. Relevant documents from the literature on financial tools used in parkland acquisition, parkland development, and park operation were also reviewed. In all, the list of key documents included:
 - RDN Bylaw 1231 (the establishing bylaw for the Regional Parks & Trails Service)
 - RDN's *2017 Five Year Financial Plan*
 - 2016 and 2017 requisition totals for the regional and community services, parcel totals, converted assessment and population data
 - various staff reports, including the "Regional Parks Parcel Tax Review" (2016), and the "Amendment of the Regional Parks Function to Include Municipalities" (2005)
 - *Regional Parks and Trails Plan* (2005-2015)
 - *Regional Parks DCC Review* (2007)
 - *Regional District of Nanaimo Strategic Plan, 2016-2020*
 - Acquisition Criteria Rating Sheet
 - *2017 RDN Operational Plan*

- *Community Parks & Trails Strategic Plan* (2014)
- studies related specifically to individual community parks and trails in the electoral areas
- all regional park management plans

- *Comparative Research* — The consultants examined the regional and local parks services in ten regional districts across British Columbia. Particular attention was paid to services in the:

- Cowichan Valley Regional District
- Regional District Central Okanagan
- Capital Regional District
- Comox Valley Regional District

Interviews were conducted with senior managers in several cases in order to fully understand the funding models in place.

- *Consultation* — The consultants held one facilitated discussion with the RDN Board of Directors, and one with the Chief Administrative Officers of the member municipalities and the Regional District.¹ For both meetings, background materials and questions for discussion were distributed in advance. Meetings were held, as well, with senior managers at the RDN in Recreation and Parks Services, Finance, and Strategic Initiatives.
- *Report and Recommendations* — The consultants prepared the report for presentation to the RDN Board of Directors.

FORMAT OF REPORT

This report presents the results of the *RDN Parks Funding Service Review*. The document is divided into two parts.

Part I: Regional Parks and Trails Service

The first and largest part of the report focuses on the Regional Parks and Trails Service. Part I is divided into five chapters:

- *Chapter I.1: Current Service* — Chapter I.1 profiles the RDN's Regional Parks & Trails Service. The profile highlights the funding model and financial tools in place today, and the changes that have occurred to service funding since the service's inception. The profile also identifies challenges facing the service.

¹ The CAO's or their designates from all member municipalities were invited. Representatives from Parksville, Qualicum Beach and Lantzville attended, along with the CAO from the RDN. Representatives from the City of Nanaimo were not available.

- *Chapter I.2: Service Funding* — Chapter I.2 outlines the full range of financial tools available to regional districts to assist in funding the acquisition, development and operation of regional parks and trails. The text draws heavily on comparative research undertaken on regional parks and trails services across British Columbia.
- *Chapter I.3: Assessment of Service Funding Models* — Chapter I.3 introduces and applies a set of evaluation criteria for assessing the RDN's Regional Parks & Trails funding model. "Equity" is a key criterion in the exercise — the terms of reference for the *Review* specifically highlight the importance of assessing equity among participating jurisdictions.
- *Chapter I.4: Additional Issues* — This chapter examines specific issues that arose over the course of the *Review*, but that do not necessarily fit into the discussions on funding models. Two issues in particular are addressed:
 - the potential for the RDN to assume responsibility, in whole or part, for municipal parks that have regional park characteristics
 - the potential for an integrated approach to parks and trails planning that would take into account municipal and electoral area park systems, along with the regional parks and trails system
- *Chapter I.5: Summary of Recommendations* — The final chapter summarizes the recommendations on the regional service funding model, and on the issues raised in Chapter I.4.

Part II: Community Parks and Trails Services

The second part of the report examines the funding model in place for the eight Community Parks and Trails Services. Chapter II.1 begins by profiling the services and their funding model. Chapter II.2 then considers the range of financial tools available to regional districts to assist in the acquisition, development and operation of parks and trails at the community level. The current funding model is assessed in Chapter II.3 using the same criteria introduced for the regional service.

Recommendations are summarized in Chapter II.4.



PART I

REGIONAL DISTRICT OF NANAIMO REGIONAL PARKS AND TRAILS SERVICE

**RDN
PARKS FUNDING
SERVICES REVIEW
REPORT**

NEILSON-WELCH
CONSULTANTS TO GOVERNMENT

**NOVEMBER 2017
PAGE 4**

CHAPTER I.1 CURRENT SERVICE

This chapter profiles the Regional Parks & Trails Service as it exists today. Several elements of the service are outlined; however, the focus is on the service's funding model.

OVERVIEW OF REGIONAL PARKS & TRAILS SERVICE

The RDN was granted authority in 1989 by way of Supplementary Letters Patent to establish and provide regional parks and trails. In 2001, the Regional District converted the function to a regional service, as provided within the framework of the *Local Government Act*. The *Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)* became the establishing bylaw for the service.

The vision for the service was first presented in a 1995 *Regional Parks System Plan*. A subsequent 2005 *Regional Parks and Trails Plan* built on and refined the earlier vision to create a four-part purpose that balances the need to protect natural areas in the region with the desire to promote access to them. As set out in the 2005 document, the *Regional Parks & Trails Service* exists to:

- secure, protect and steward land and water features of environmental significance and wildlife habitat value
- provide rewarding outdoor recreation opportunities
- foster education on and appreciation of the Region's natural environment
- enhance livability for current and future residents of the RDN

The RDN undertakes all facets of regional parks and trails service provision, including system planning, land acquisition, the establishment of management plans for individual regional parks and trails, regional parks and trail development, and the ongoing operation of parks and trails in the system. Land acquisition efforts are guided by acquisition plans and goals, acquisition criteria, and a scoring tool to assist decision-makers in making selections. The management and development of each regional park are governed by a park-specific management plan.²

The Regional Parks & Trails Service today is a true regional service that includes all jurisdictions of the RDN as participants. Full participation, however, has not always been a feature of the service. For the first decade of its existence the regional service received support from the electoral areas only. In late 2000, as the result of a multi-service *Regional Services Review*, the RDN's member municipalities entered into a *Regional Parks Service Agreement* with the RDN to contribute towards the operation and maintenance (but not the acquisition or capital development) of

² The management plan for Beachcomber Regional Park is under development. All other regional parks have plans in place.

regional parks on a per capita basis. In 2006, following a review of the *Agreement*, and in response to increasing demand across the region for large natural parks and trails, the municipalities joined the regional service as participants, and began to contribute to land acquisition and development efforts.

The expansion of the regional service to include all jurisdictions enabled the network of regional parks and trails to grow significantly between 2006 and 2017. In 2006, the system consisted of eight parks, with a total area of 430 ha. Trail development had occurred to the point that by 2006, there were 60 km of regional trails in the system. Today, at the time of writing, the system consists of 12 regional parks covering a total of 2,129 ha, and a network of regional trails approaching 90 km in length.³

FUNDING MODEL

For the purpose of this report, the term "funding model" focuses on the financial tools used by the Regional District to pay the different costs of the service, including costs associated with land acquisition, parks and trails capital development, and planning, operations and maintenance.

Land Acquisition

In the RDN, as in all other regional districts with regional parks services, lands are acquired for regional parks and trails in two basic ways: through direct purchase by the Regional District; and through transfer to the Regional District by others.

► Direct Purchase

Direct purchase is an important element of the RDN's acquisition efforts. As in most regional districts, the RDN relies on property tax revenues to fund its purchases. Unlike other regional districts, however, the RDN relies solely on a property parcel tax to raise acquisition funds. Each year, all property owners in the Regional District pay a flat tax for each parcel of land.⁴ The total amount contributed to the service from each participating jurisdiction equals the number of parcels in the jurisdiction, multiplied by the flat parcel tax. The funds raised through the parcel tax are placed into a Regional Parks Acquisition and Capital Development Fund. Monies in the fund are used primarily to purchase lands; however, resources are also used where required to fund major capital improvements in the system, such as bridges and parking areas.

³ Much of the growth in hectares can be attributed to two specific regional parks acquired since 2005, including Mount Benson Regional Park (212 ha) and Mount Arrowsmith Massif Regional Park (1,300 ha).

⁴ To be identified as a parcel for the purposes of taxation a separate tax folio must exist. Rental apartments and individual manufactured homes within manufactured home parks or mobile home parks do not have separate folios. These units are not, therefore, considered parcels, and are not charged the parcel tax.

Prior to 2006, member municipalities were not participants in the regional service. As noted earlier, they did contribute beginning in 2001 to operations and maintenance through a *Regional Parks Service Agreement*. This *Agreement*, however, did not allow for contributions to land acquisition. The flat parcel tax, as the chosen method for funding acquisition, was introduced when the municipalities entered the service.

When it began in 2006, the flat parcel tax was set at a rate of \$10.⁵ The rate remained at this level until 2011 when it was increased to \$11. Between 2011 and 2016, the rate climbed from \$11 to \$14, always remaining a flat, per-parcel amount. In 2016, parcel tax revenues totaled \$950,000; budgeted revenues for 2017 are at essentially the same level. Figure I.1.1 on the following page shows the parcel tax contributions from each jurisdiction in 2017. Also shown for each is the number of parcels.

➤ **Transfer of Lands**

Where possible, lands are acquired by the Regional District through transfers from senior levels of government, non-profit societies, private corporations and, in some cases, individuals. The RDN has secured a number of land transfers from the provincial government in past years. In some instances title of ownership was transferred, as when the province transferred 105 ha of land to create Horne Lake Regional Park. In other instances, the province granted long-term operating leases to the Regional District, or licenses of occupation. The long-term lease of 22 ha at Benson Creek Falls Regional Creek, and the license of occupation granted to the RDN over 1,300 ha in Mount Arrowsmith Massif Regional Park, are examples.

Several regional parks have been established with the help of contributions from the Nanaimo & Area Land Trust (NALT), the Land Conservancy of BC, the Nature Trust of BC, the Nature Conservancy of Canada, Ducks Unlimited and others. These contributions typically take the form of long-term leases or licenses of occupation. Land contributions from private corporations represent an additional tool — Timber West is one corporation that has contributed lands in past years to the regional parks system (e.g., Englishman River). Contributions in the form of gifts from individuals, while less common, do occur periodically. Coats Marsh Regional Park, Beachcomber Regional Park and Little Qualicum River Regional Park were all established, in part, using lands gifted by individuals.

⁵ The parcel tax applied to the electoral areas and the City of Nanaimo in 2006, but was phased in for the other municipalities over five years. The City of Nanaimo began paying in 2006 to support the acquisition of Mount Benson Regional Park, which the City had identified as a priority.

**Figure I.1.1
Parcel Tax (Acquisitions) and Value Tax (Operations)
2017 Requisitions**

Jurisdiction	No. Parcels	Pop (2016)	Parcel Tax Rate	Value Tax Rate	Requisition		
					Acquisition	Operations	Total
City of Nanaimo	34,369	90,504	14.00	0.041	481,166	797,168	1,278,334
City of Parksville	6,737	12,514	14.00	0.036	94,318	110,225	204,543
Town of Qualicum Beach	4,841	8,943	14.00	0.032	67,774	78,771	146,545
District of Lantzville	1,482	3,605	14.00	0.036	20,748	31,753	52,501
Electoral Area A	2,902	7,058	14.00	0.047	40,628	62,168	102,796
Electoral Area B	3,771	4,033	14.00	0.029	52,794	35,523	88,317
Electoral Area C	1,425	2,808	14.00	0.022	19,950	24,733	44,683
Electoral Area E	3,423	6,125	14.00	0.025	47,922	53,950	101,872
Electoral Area F	2,994	7,724	14.00	0.048	41,916	68,034	109,950
Electoral Area G	3,729	7,465	14.00	0.035	52,206	65,752	117,958
Electoral Area H	2,525	3,884	14.00	0.030	35,350	34,210	69,560
Total Requisition					954,772	1,362,287	2,317,059

The Value Tax Rate differs for each jurisdiction because costs for operations are allocated among participating jurisdictions on the basis of population, then collected from individual property owners on the basis of assessment. If costs for operations were allocated and collected based on assessment, the tax rate would be the same.

The transfer of lands through the development process is most commonly used to acquire small parcels of land for community parks. Opportunities also exist at the regional level from time to time, however, to secure land transfers at subdivision or through rezoning. In the RDN at present, a 100 ha parcel of land is being dedicated pursuant to a 20-year phased development agreement to create a regional park in the Fairwinds' Lakes District Neighbourhood (Area E). In 2001, the initial 44 ha Little Qualicum River Regional Park was acquired through dedication at subdivision (later, in 2017, an additional 68 ha was added to the Regional Park through a land donation).

► **Combination of Methods**

It is useful to note that in the RDN, as in other regional districts, regional parks and trails are typically established, or enhanced, using a combination of direct purchases and land transfers. Direct purchases by the RDN are often used to leverage transfers from other agencies that share the Regional District's vision for a particular site.

Parks and Trails Development

Capital projects that are undertaken to develop regional parks and trails include trail improvements, parking areas, washroom facilities, ecosystem protection works, bridges, and other similar works. Major projects are funded through contributions

from the Regional Parks Acquisition and Capital Development Fund, senior government grants (e.g., Federal Gas Tax Sharing),⁶ and operating revenues raised through property value taxes. Capital contributions obtained through the development process are secured in specific cases (e.g., Fairwinds' Lakes District Neighbourhood), but are not common.

Contributions to regional trail development are provided, in some cases, by individual member municipalities and electoral areas that comprise the RDN. For example, Electoral Areas F and G contributed Community Works Fund (CWF) grant monies in 2016 and 2017 to assist in the cost of developing the portions of the E&N Rail Regional Trail that traverse the two Areas. Contributions to development costs come, as well, from community groups that raise funds to assist with specific projects.

Planning, Operations and Maintenance

The RDN raises funds to pay for regional park planning, operations and maintenance using a property value tax. The service costs that are paid using the tax are allocated among participating jurisdictions on the basis of population. The tax, however, is applied to property owners based on assessment. Total tax revenues collected in 2016 were \$1.34 million; 2017 revenues are 2.1% higher at \$1.36 million. The property value tax contributions from the service participants, along with population data, are provided in Figure I.1.1 (page 8).

Challenges Related to Funding Model

The RDN's Regional Parks and Trails Service is facing three key challenges related to funding: increasing demand for the service; rising land values and capital costs; and equity among jurisdictions.

➤ Demand for the Service

The regional parks and trails service in the RDN, similar to services in other parts of the province, provides many benefits to the region and its residents. For example, the service:

- helps to protect, in perpetuity, important natural features, ecosystems and habitats, some of which may be threatened
- offers opportunities to residents and visitors to connect with, learn about, and be active in outdoor, natural environments
- provides a range of ecosystem services to the broader community in the form of improved air quality, nutrient recycling, flood regulation, water supply and treatment, and other benefits

⁶ In 2014, the RDN applied for and received \$2.6 million from the Regionally Significant Priorities Gas Tax under the Federal Gas Tax Sharing program. The funds were used to construct the Coombs to Parksville Rail Trail. Projects that receive these grants are deemed to provide broad, regional benefit.

- draws visitors to the region, and in so doing assists in economic development efforts

The significance of these benefits and the service that provides them tends to rise in tandem with population growth and development. In parts of the province, such as the RDN, that experience sustained growth and urbanization, people become increasingly aware of the importance of protected natural areas and the desire to connect with nature. With increasing awareness comes increasing demand to expand and develop the regional parks and trails system.

The RDN does not yet have good data on the volume of visits to its different regional parks and trails (numbers are beginning to be tracked); nor has the Regional District attempted to measure demand through surveys or other qualitative means. It is difficult, therefore, to state definitively that demand for the service is rising. In the discussion with Board Directors, however, the need to grow the service was clear. Directors highlighted the dual desire to protect additional natural areas through acquisition, and to make available existing and new regional parks and trails to growing populations through development.

The park development piece is important to emphasize. Directors on the whole gave voice to the expectation that regional parks acquired by the RDN should be made accessible to the residents of the RDN. Staff echoed this point in separate discussions, and referred to expectations from a growing variety of user groups, including mountain bike clubs, kayak and diving groups, and others.

➤ **Land Values and Capital Costs**

The cost of land on the East Coast of Central Vancouver Island continues to experience upward pressure, as data from BC Assessment help to illustrate. Cost pressures are attributable to a number of factors, including general growth in the region, and the limited supply of land available for acquisition relative to other parts of the province. Regardless of the causes, increasing land values make land acquisition through purchase difficult to pursue.

To date, the RDN has been successful in securing a considerable amount of its lands through partnerships and contributions; efforts to develop new partnerships and attract additional contributions will surely continue. To leverage contributions and to enter into partnerships, however, the RDN needs to have its own funds on hand. As land costs increase, so too does the pressure on existing acquisition reserves, and the demand for new funding sources.

The cost to develop capital infrastructure in regional parks is also facing upward pressure, over-and-above the Consumer Price Index rate of inflation.

Staff note that in 2017 the RDN had to postpone certain capital projects because of higher-than-anticipated contractor bids. Managers from other regional parks systems who were interviewed for the *Service Review* — RDCO, CRD and MVRD are examples — are experiencing the same issue. Costs are escalating as a result of rising material and contractor costs.

➤ **Equity**

In any shared service, ensuring a level of equity between and among members is an ongoing challenge — the RDN's Regional Parks & Trails Service is no exception. An assessment of equity under the current funding model is provided later in Chapter I.3.

CHAPTER I.2 FINANCIAL TOOLS

This chapter reviews the range of tools available to regional districts in British Columbia to fund the various activities that are undertaken in regional parks and trails services. Most of the tools are in use already at the RDN. The information presented draws heavily on the comparative research that was conducted for the *Service Review*.

COMPARATIVE RESEARCH

Pursuant to the *Service Review's* terms of reference, the consultants undertook comparative research on regional parks and trails services across BC. Materials were reviewed and, in several cases, managers were interviewed, from a total of ten regional districts, including:

- Cowichan Valley Regional District (CVRD)
- Capital Regional District (CRD)
- Comox Valley Regional District (Comox Valley RD)
- Regional District Central Okanagan (RDCO)
- Metro Vancouver Regional District (MVRD)
- Regional District Okanagan Similkameen (RDOS)
- Powell River Regional District (PRRD)
- Fraser-Fort George Regional District (FFGRD)
- Regional District East Kootenay (RDEK)
- Regional District Central Kootenay (RDCK)

The consultants gathered information on each regional district's service, size of regional parks system, evolution of the system, and challenges being faced today. Special attention was paid to service funding — more specifically, the tools being used in each regional district to pay for regional parks and trails acquisition, development, and planning, operation and maintenance.

In general, the comparative research confirmed that the range of tools available to regional districts to fund regional parks and trails services is limited. The research also revealed that the primary financial tool used across regional districts to pay for the services is property value taxes. This finding was not unexpected given the nature of regional parks and trails as true public good services.

FINANCIAL TOOLS

Financial tools are identified under each of the main service components, namely land acquisition, parks and trails development, and planning, operations and maintenance.

Land Acquisition

As noted earlier, regional districts acquire land for regional parks and trails in two different ways: through direct purchase, and through transfer to the regional district by others. Figure I.2.1 presents the different tools available to regional districts under each of these approaches.

Figure I.2.1
Land Acquisition Tools

TOOL	DESCRIPTION
Direct Purchase of Land	
Property Value Tax	<p>A property value tax is a tax levied on the assessed value of properties within a service area to raise the revenue necessary to fund the cost of a service. The cost may be allocated among participating jurisdictions on the basis of converted assessment, population, or any other factor or combination of factors. If cost is allocated on the basis of converted assessment, the value tax rate will be uniform throughout the service area.⁷ If cost is allocated on some other basis, such as population, the value tax rate that is applied to collect revenues will vary by jurisdiction. Within each jurisdiction, the rate — however it is determined — will be levied against the assessed value of each property.</p> <p>All regional districts surveyed, with the exception of the Comox Valley RD, use a property value tax to raise funds for land acquisition.⁸ In every case, the value tax is levied against the full assessed value of properties — that is, the value of land and improvements.</p> <p>The amount of tax paid by each property varies based on assessed value. In some of the regional districts, the payment is communicated in information materials as a standard dollar amount per household. The CRD and CVRD, for example, both identify a per-household rate of \$20. This amount, however, reflects the payment that a household with an average residential assessment pays through the property value tax towards acquisition. The actual amount paid by any particular household varies depending on the assessed value of the household relative to the average value in the service area.</p> <p>Most regional districts have land acquisition reserve funds in place to hold the property tax revenues collected for acquisition. These funds promote transparency, ensure that the monies are used for their intended purpose, and help to raise awareness of the importance of ongoing acquisition in regional parks and trails systems. In some cases, regional districts create their acquisition funds within the existing regional parks and trail service — RDCO and MVRD are examples. In</p>

⁷ Allocation on the basis of converted assessment is the default under the *Local Government Act*.

⁸ The Comox Valley service is a sub-regional service in that it does not include the Regional District's member municipalities.

RDN
PARKS FUNDING
SERVICES REVIEW
REPORT

NEILSON-WELCH
CONSULTANTS TO GOVERNMENT

TOOL	DESCRIPTION
	<p>these regions, a specific portion of the total tax revenue collected is transferred to the reserve fund. In a few cases — the CRD and CVRD stand out — the regional districts have established separate land acquisition services, with separate value taxes in place, to collect and hold the monies.</p> <p>In the RDOS, RDEK and RDCK, separate services have been established with separate value taxes to collect funds specifically for conservation lands.</p>
Property Parcel Tax	<p>A property parcel tax is levied against each parcel of property in an amount that is not linked to the assessed value of the property. The tax may be a flat tax — i.e., a specific, common dollar amount that is levied against each property. Alternatively, the tax may vary based on the size of property, or the length of frontage.</p> <p>Other than the Comox Valley RD's flat parcel tax, levied for its sub-regional parks service, the RDN is the only regional district in the comparison group that uses a parcel tax to fund land acquisition. The rate per property in the Comox Valley is \$20; the rate in the RDN is \$14.</p>
Development Cost Charges	<p>Development in a regional district results in an increased demand for various regional services, including regional parks and trails. Regional districts have the authority under the <i>Local Government Act</i> to impose development cost charges (DCCs) on new development to recover the portion of the acquisition cost that has been incurred, or that will be incurred, to meet the demand for regional parks and trails generated by new development.</p> <p>Several regional districts in the comparison group charge DCCs to assist in providing regional infrastructure services (e.g., sewer trunk lines and treatment plants). No regional district, however, charges DCCs to assist in the acquisition of lands for regional parks and trails services.</p> <p>In 2011, the RDN came close to implementing the first regional park DCC in BC. A staff report at the time estimated, based on a 2007 consultant's study, that DCCs could help the RDN to collect significant funds over a 30 year period — \$19 million to \$24 million — to assist with the acquisition and development of lands for the regional parks and trails system. The proposed DCC bylaw that was presented did not, however, receive final Board approval.</p>
Transfer of Funds	<p>While not a significant source of revenue, regional districts may receive funds from other agencies towards the purchase of specific properties. In the RDN, the Nature Trust of BC and NALT together raised \$156,000 in 2011 towards the RDN's \$4.8 million purchase of lands for Moorecroft Regional Park. Contributions resulting from fundraising efforts and other initiatives are more typically directed to capital projects.</p>

TOOL	DESCRIPTION
Transfer of Land	
Transfers from Government	<p>Lands suitable for inclusion in regional parks and trails services may be transferred to a regional district, at no cost, by other governments. Transfers may involve the transfer of ownership (i.e., title) over lands, or the transfer of responsibility for lands through long-term leases, licenses of occupation, or other mechanisms. Where ownership is transferred, covenants may be attached to ensure that lands retain their parkland nature. Terms included in leases and licenses of occupation provide the same protection.</p> <p>Regional districts have traditionally relied on the transfer of provincial Crown lands to establish and expand regional parks and trails systems. Today, however, land transfers from the province are less common than before in most parts of the province. The change is attributable, in part, to the need to take into consideration and consult on First Nations' interests in the provincial lands. The change is also attributable in some regions to a decline in the amount of suitable provincial land.</p> <p>Transfers of federal Crown land are less common than those from the province, and face the same challenges related to consultation and lack of supply. Federal transfers do, however, remain a tool to consider, particularly in the form of long-term management leases. The MVRD, among others has leases in place in some of its regional parks.</p>
Transfers from Non-Profit Agencies	<p>All regional districts secure lands for regional parks and trails services through contributions from non-profit societies that exist to protect lands, ecosystems and natural habitats from development.</p> <p>As noted in Chapter I.1, the RDN has several partnerships in place with groups such as NALT, the Land Conservancy of BC, the Nature Trust of BC, the Nature Conservancy of Canada, Ducks Unlimited and others. Transfers from these groups usually occur through long-term management leases or licenses of occupation, so that actual ownership remains with the contributor.</p> <p>Contributions from non-profit groups are often combined with direct purchases of lands by regional districts. In this way, the regional district funds may be seen to leverage investments by others in order to create more extensive regional parks than would otherwise be possible.</p>
Transfers from Other Agencies	<p>Resource companies and others that own large tracts of lands will, at times, transfer ownership of properties, or grant licenses of occupations or rights-of-ways, to regional districts for use as regional parks or trails. In some cases, transfers of ownership may be made to a non-profit agency, which then makes the land available to the regional district for operation through a regional parks and trails service.</p>

TOOL	DESCRIPTION
	In future years in the RDN and in other regional districts, the need for rights-of-way through private lands is anticipated to grow, as demand for new regional trails grows.
Transfers from Individuals	<p>Gifts from individual landowners are another form of land transfer that benefits regional parks and trails services. Individuals are typically eligible to receive tax credits for donations of land that are made.</p> <p>In some cases, individuals may sell lands to regional districts at discounted, below-market rates. In all cases, it is common for covenants to be attached to lands in order to ensure their use as regional parks in perpetuity.</p>
Parkland Transfers at Development	<p>Regional districts can receive property through the development process in the form of land dedications and contributions. Dedications at subdivision, pursuant to section 510 of the <i>Local Government Act</i>, are typically used for community parks services, but may be used for regional parks as well. Contributions provided by developers during the rezoning process, and through phased development agreements, may also benefit regional services. The examples in the RDN of such contributions were identified earlier in Chapter I.1.</p> <p>Municipalities may also use the development process to acquire lands for transfer (ownership or lease) to a regional districts. In such cases, the lands received by the municipality would have regional park characteristics, including a large benefitting area.</p>

➤ A Note on Borrowing

Short- and long-term borrowing are cited by some regional districts as financial tools for use in the acquisition of regional park lands. Both forms of borrowing are, indeed, used by regional districts for acquisition, most often in cases where the amount of funds in reserve are insufficient to take advantage of opportunities that have arisen to purchase desired properties. Short-term borrowing may be undertaken for up to five years without the assent of electors. Long-term loans may have much longer amortization periods, but may require elector assent.⁹

Despite their use, short-term borrowing and long-term borrowing are not considered acquisition tools in this report. In the context of land acquisition, borrowing is essentially a cash-flow management tool that can be used by regional districts to make expenditures before revenues from property taxes and/or DCCs are fully collected. Borrowing may allow regional districts to

⁹ At the RDN, long-term borrowing (20 years) assisted in the acquisition of lands for Moorecroft Regional Park (elector assent was not required as the total outstanding amount of borrowing did not exceed \$5 per thousand dollars of net taxable value of land and improvements). Short-term borrowing is used regularly as required.

acquire lands sooner than they could under a pay-as-you-go system of service funding; but borrowing does not constitute a new source of funding separate from property taxes and DCCs.

Parks and Trails Development

Figure I.2.2 presents the different tools available to regional districts to undertake capital projects in regional parks and trails services.

Figure I.2.2
Parks and Trails Development Tools

Tools	Description
Grants from Senior Governments	<p>All regional districts rely on senior government grants to assist in the cost of infrastructure development in regional parks and trails. Grants under the Strategic Priorities Fund (Federal Gas Tax Sharing program), in particular, are pursued and obtained where possible.</p> <p>Other one-time grant programs are also pursued where available, such as the recent Canada 150 Community Infrastructure Program,¹⁰ and the Federation of Canadian Municipalities Green Municipal Fund.</p>
Contributions from Non-Profit Agencies	<p>All regional districts also rely on contributions from regional non-profit societies to assist in the funding of specific works that tend to be selected by the societies based on their particular missions. Metro Vancouver, for example, depends on the Pacific Parklands Foundation to assist in environmental works and other capital projects that promote the Foundation's goals. MVRD, RDCO and most other regional districts rely, too, on regional park associations to raise money for improvements in the specific parks. The RDN has received contributions from, and has benefitted from the fundraising efforts of, non-profit groups across the region.</p>
Land Acquisition Reserve Funds	<p>Rising expectations and costs related to parks and trails development force some regional districts to make use of property tax revenues that are raised, either through parcel or value taxes, for land acquisition. Accessing acquisition funds for development purposes, however, is difficult in several cases, need notwithstanding. In the CRD, for example, gaining access to acquisition funds may require a bylaw change and elector assent. The MVRD is facing the same constraints, as is the CVRD with its separate acquisition service. RDCO and the RDN are reportedly less restricted in their use of acquisition reserve funds for major capital works.</p>
Property Taxes	<p>Property tax revenues that are collected to pay for service operations are used in most (if not all) regional districts to assist with capital development.</p>

¹⁰ The Regional District of Okanagan-Similkameen received funding under this program for regional trails.

Tools	Description
	A unique approach to the use of property taxes for capital works exists in the Cowichan Valley. The CVRD has established a separate service — the Kinsol Trestle service — to raise property tax revenues specifically for use in reconstructing and maintaining the Kinsol Trestle. This approach was taken to ensure strong support for the project, to promote transparency, and to raise the dedicated (and significant) funds required.
Development Cost Charges	The authority of regional districts to impose DCCs for regional park land acquisition was noted in Figure I.2.1. The same authority allows regional districts to use DCCs for regional parks and trails development costs. As with land acquisition, no regional district currently uses, or has ever used, DCCs for regional parks and trails development.
Other	Some regional districts — RDCO and MVRD are current examples — have memorial and other programs that allow individuals to provide funds for benches, picnic tables and similar types of infrastructure. Regional districts may also work with individuals who wish to gift funds (as opposed to lands) for specific works. Monies raised through these initiatives tend to be limited.

Planning, Operations and Maintenance

Figure I.2.3 presents the different tools available to regional districts to fund regional parks and trails planning, operations and maintenance.

Figure I.2.3
Planning, Operations and Maintenance Tools

Tools	Description
Property Taxes	Property value taxes are the primary tool used by every regional district to pay for the operation and maintenance of regional parks.
User Fees	<p>User fee revenues, generated from film permits, special event permits, commercial licenses and other special-use permits, are a secondary source. In all cases, however, user fee revenues are modest at best. Indeed, only three of the regional districts studied for the <i>Service Review</i> — the CRD, RDEK and MVRD — appear able to generate in excess of 5% of total service revenues from user fees.</p> <p>This level of funding is not unexpected given the nature of regional parks and trails. They are designed to provide access to all residents, free of financial and other barriers. Fees for parking and other services have been considered by some regional districts; ultimately, however, such fees were rejected for fear that they would prevent some groups of residents from using parks and trails.</p>

CHAPTER I.3

ASSESSMENT OF FUNDING MODEL

This chapter provides an assessment of the RDN's funding model that is in place today to pay for the Regional Parks & Trails Service. Recommendations for the Board to consider are put forward. The experiences of other regional districts, identified through the comparative research, inform both the assessment and the recommendations.

EVALUATION CRITERIA

As noted in the terms of reference for the *Service Review*, equity among service participants is particularly important as an evaluation criterion. Other criteria, however, are also important to consider. The full list of criteria used in this report includes:

- *Equity (Jurisdictions)* — All member jurisdictions of the RDN — municipalities and electoral areas — are participants in the regional service. Is the service's current funding model fair to all parties? Are there changes to the model, based on approaches taken elsewhere, that would make the system more equitable on the whole?
- *Equity (Individual Taxpayers)* — Is the current funding model, with its reliance on both property value taxes and a flat-rate property parcel tax, fair to the different types of taxpayers who benefit from and pay for the service? Could the model be improved?
- *Effectiveness* — Does the current funding model allow the RDN to raise sufficient revenue for the service, given expectations and key challenges?
- *Transparency* — Is the current funding model clear in communicating to taxpayers and jurisdictions the purposes of monies that are raised?

REGIONAL PARKS & TRAILS FUNDING MODEL

The RDN's Regional Parks & Trails Service was profiled in Chapter I.1. The service's funding model can be summarized by the following points:

- Lands for regional parks and trails are acquired through direct purchase by the Regional District, and through transfer to the Regional District by others. The two methods of acquisition are often used in combination.
- The RDN relies solely on a property parcel tax to raise resources to purchase lands. The parcel tax is a flat tax, in that it is the same amount (\$14) for each parcel, irrespective of the parcel's assessed value. The tax has been increased four times since its introduction in 2006 at \$10.

- Land transfers are pursued from and received by senior levels of government, non-profit societies (e.g., land trusts), private corporations and individuals.
- Capital projects undertaken to make regional parks and trails accessible to users are funded through contributions from the Regional Park Acquisition and Capital Development Fund, senior government capital grants, contributions secured through the development process, and operating revenues that are raised using a property value tax. Funding for regional trails, in particular, is provided in some cases by individual member jurisdictions for the portions of trails that traverse their areas.
- Funds for planning, operations and maintenance are raised using a property value tax. The service costs that the tax is used to fund are allocated among jurisdictions on the basis of population.

ASSESSMENT OF REGIONAL SERVICE FUNDING MODEL

Equity (Jurisdictions)

To assess the funding model's level of equity — or fairness — among jurisdictions, it is useful to consider the benefits received by the different jurisdictions, and the cost of participation in the service for the different jurisdictions.

► Benefits

The *Regional Parks & Trails Service* provides broad, indirect benefits to the region as a whole, including:

- protection, in perpetuity, of important natural features, sensitive ecosystems, landscapes and habitats in the region
- ecosystem services in the form of improved air quality, nutrient recycling, flood regulation, and water supply and purification

Residents in all jurisdictions of the Regional District receive these important, indirect benefits from the service, irrespective of the residents' ability to access the regional parks and trails in the system. Put differently, all residents in the RDN, whether or not they are able to visit regional parks and trails, benefit from efforts to protect the region's natural environment and ecosystem services.

The *Regional Parks & Trails Plan* (2005-2015) recognizes the value of these indirect benefits provided by the service. The protection of natural areas, landscapes, ecosystems and habitats is featured prominently in the vision for the service. The *Plan* also, however, points to the importance of direct benefits to residents who are able to use the regional parks and trails. As set out in the *Plan*, the service exists in part to provide opportunities to residents and visitors to access, learn about, and be active in outdoor natural

environments. The service also is designed to draw visitors to the region, and in so doing to promote economic development. These direct benefits exist for jurisdictions in which residents and visitors are able to easily access and make use of the parks and trails.

Indirect benefits by their very nature are difficult to measure for the service area as a whole, but also for individual jurisdictions within the area. It may be helpful simply to acknowledge that, through its targeting of different landscapes and habitats, and in its efforts to protect key ecosystems, the service provides broad, indirect benefits to the entire region.

Direct benefits are also not easy to measure, but may be approximated in different ways:

- *Actual Usage* — The estimated number of visits to regional parks and trails by residents of different jurisdictions can be used to judge direct benefit. Unfortunately, the RDN does not yet track visitor numbers to its different properties, nor does it conduct periodic surveys to identify the home jurisdiction of different users.
- *Population* — For several local government services, population is considered a proxy measure for usage, and one way to gauge direct service benefit. Population, arguably, is particularly well-suited to parks and trails services which are designed, in part, to be accessed and used by people.
- *Proximity of Regional Parks & Trails* — The proximity of regional parks and trails to individual jurisdictions can be used to assess the level of system access available to residents in each jurisdiction. Figure I.3.1 presents data from the RDN to show the number of regional parks and trails within 60 minutes' driving time, 45 minutes' driving time, and 30 minutes' driving time from a central location in each jurisdiction. The information in the figure shows that, on the whole, access to the regional parks and trails system is uniformly strong for most jurisdictions at the 60 and 40 minute marks (the exception is Gabriola Island which has less access relative to other jurisdictions on account of the need for ferry travel). Access at the 30 minute mark, however, is considerably better for jurisdictions in the north of the RDN (District 69) than in the south, as measured by number of regional parks within easy reach.
- *Expenditures* — It may be argued that spending decisions of the RDN benefit, or have the potential to benefit, different jurisdictions depending on the location of the expenditures. Under this argument, spending of service funds to acquire, develop or operate

**Figure I.3.1
Proximity to Regional Parks and Trails
Driving Times**

Jurisdiction	Place	Number of Parks & Trails Within		
		60 Min.	45 Min.	30 min.
Nanaimo	Nanaimo City Hall	15	10	4
Lantzville	Lantzville District Hall	15	14	9
Parksville	Parksville City Hall	14	14	10
Qualicum Beach	Qualicum Beach City Hall	14	13	10
Area A	Cedar Plaza	14	9	4
Area B*	Gabriola Island Ferry Terminal	6	2	2
Area C	Extension	15	8	4
Area C	East Wellington	16	12	8
Area E	Nanoose Place	14	14	12
Area F	Coombs Country Market	14	14	11
Area G	Sunrise Dr. at Island Highway (19A)	14	13	9
Area H	Lighthouse Community Centre	14	11	8

* Includes ferry travel time.

specific regional parks and trails will benefit the jurisdictions closest to the properties.

Detailed operating and capital budgets (2016 and 2017) were reviewed for the assignment, along with land acquisition guides and criteria, to gauge the fairness of spending in the service. No spending patterns were identified to suggest any disproportionate level of benefit to individual participants. Spending on acquisition is guided by Board-endorsed criteria, including one criterion that calls for "geographical equity". This criterion states that balance between and among electoral areas and sub-regions is an important outcome for the Board.¹¹

Spending on development is modest, given the nature of the service, except in cases where bridges and parking areas must be constructed. Examples of these major capital works exist in both major sub-regions, demonstrating again the sensitivity shown to spatial equity. Spending on operating is also dispersed across the region as shown by annual work plans.

¹¹ Electoral Areas are identified specifically for two reasons: they are spread throughout the entire Regional District; and candidate properties for acquisition are most often located in the rural areas where land costs are (usually) lower relative to those in the municipalities, and where large natural areas of regional significance tend to be situated.

➤ Participant Costs

The allocation of acquisition and development costs among participating jurisdictions based on number of parcels is not common in regional district funding models for regional parks and trails. Number of parcels does not take into account differences across jurisdictions in total converted assessment. Converted assessment, as a measure, is widely considered to reflect a jurisdiction's ability to pay; allocation of costs on the basis of converted assessment is accepted as the fairest approach for cost sharing in services that provide broad, indirect benefits.

Allocation of costs for planning, operations and maintenance on the basis of population is another relatively unique approach for regional parks and trails services specifically. As a proxy measure for service usage, population is used to allocate costs in cases where level of service usage is considered important, but where data on actual usage do not exist. The reliance on population places considerable value on the direct benefits of the Regional Parks & Trails Service to residents. The important indirect benefits to the region as a whole that are related to the protection of natural areas and ecosystems receive less emphasis under this approach.

Across British Columbia, the full costs — acquisition, development, planning, operations and maintenance — in most if not all regional parks and trails services are allocated among participating jurisdictions on the basis of converted assessment alone. This basis, as noted, recognizes the indirect, broad benefits of the service, and is considered by many to reflect each jurisdiction's ability to pay for the service. The reliance on converted assessment entirely, however, may not sufficiently recognize the direct benefits of the service. These benefits are identified in the RDN's materials as being important. They were also recognized as important during the *Service Review* discussion with the Board.

An approach that allocates all service costs among participating jurisdictions on a combination (50-50) of converted assessment and population would recognize both the indirect benefits and the direct benefits that the Regional Parks & Trails Service is designed to provide. In the RDN, this approach would be bolstered by the general level of parity in access to the regional parks system, and in expenditures across the region.

➤ Conclusion

The discussions on benefits provided to jurisdictions and costs allocated to jurisdictions under the current Regional Parks & Trails Service funding model suggest that the current funding could be made fairer. Specifically, allocation across jurisdictions of acquisition and development costs, as well as costs related to planning, operations and maintenance, on a combination (50-50) of converted assessment and population would increase inter-jurisdictional equity. This approach would recognize and balance the

service's indirect benefits to the region as a whole, and the direct benefits to each jurisdiction.

Figure I.3.2 uses 2017 data to show how this change would impact each jurisdiction in actual dollar terms. As evident from the figure, most jurisdictions would face relatively modest change.

Equity (Individual Taxpayers)

The RDN's use of a flat parcel tax to raise the allocated funds for land acquisition represents a unique approach to taxation in regional parks and trails services, and a unique use of the parcel tax tool. Parcel taxes, in general, are used to assist in funding major infrastructure costs associated with local government utilities — for example, the construction and replacement of a water or sewage treatment plant. These utilities provide direct benefit only to properties that are physically connected to the systems, or that have the ability (but choose not) to physically connect. Put differently, local government utilities "exclude" properties that cannot connect to the services. This characteristic of exclusion is considered a "private good" attribute. Parcel taxes are considered a useful and equitable tool to assist in the funding of local services with private good characteristics.

Regional parks and trails are pure public good services. They are designed to provide access to all (i.e., to exclude none), and to benefit everyone. Such services, it is generally acknowledged, are most equitably funded using property value taxes.

Figure I.3.2
Impact of Allocating All Costs by
Converted Assessment and Population (50-50)

Jurisdiction	Existing Model			Converted Assess & Population (50-50)			Change in Allocation
	Acquisit/Dev	Ops	Total	Acquisit/Dev	Ops	Total	
City of Nanaimo	481,166	797,168	1,278,334	536,770	765,873	1,302,643	24,309
City of Parksville	94,318	110,225	204,543	78,963	112,666	191,630	(12,913)
Town of Qualicum	67,774	78,771	146,545	60,171	85,854	146,025	(520)
District of Lantzville	20,748	31,753	52,501	22,941	32,732	55,673	3,172
Electoral Area A	40,628	62,168	102,796	39,278	56,042	95,320	(7,476)
Electoral Area B	52,794	35,523	88,317	28,822	41,124	69,945	(18,372)
Electoral Area C	19,950	24,733	44,683	23,299	33,243	56,542	11,859
Electoral Area E	47,922	53,950	101,872	47,103	67,208	114,311	12,439
Electoral Area F	41,916	68,034	109,950	42,767	61,020	103,787	(6,163)
Electoral Area G	52,206	65,752	117,958	47,741	68,118	115,859	(2,099)
Electoral Area H	35,350	34,210	69,560	26,918	38,406	65,324	(4,236)
Total Requisition	954,772	1,362,287	2,317,059	954,772	1,362,287	2,317,059	-

Figure I.3.2 shows that allocating all costs on a combination of converted assessment and population would shift slightly the overall cost burden among jurisdictions. The Existing Model allocates acquisition and development costs based on number of parcels, and operating costs based on population.

Value taxes differentiate among individual properties on the basis of assessed value, which is considered a measure of a property owner's ability to pay. Owners of properties with higher than average assessed values within a service area are expected to pay more towards the cost of the service than are owners of properties with lower than average assessments. In this way, property value taxes are considered progressive. Flat parcel taxes, conversely, would be considered by many to be regressive.

The difficulty with the flat parcel tax is exacerbated further by the fact that all parcels, regardless of property class, are charged the same rate. In a value tax system, Class 4 (Major Industry), Class 5 (Light Industry) and Class 6 (Business) properties would pay different (higher) rates than Class 1 (residential) properties.

Equity as it relates to individual taxpayers needs to also consider whether there are different groups of stakeholders who contribute to the demand for the service, and who stand to benefit from the service, but who do not share in the cost of the service under the current model. The one stakeholder group that stands out at present is development. As noted earlier in the report, new development adds to the demand for new regional parks and trails in the RDN. Under the current funding model, however, there is no mechanism in place to require new development to contribute funding for additional acquisition and development of lands. The introduction of a DCC to assist with acquisition and development costs would make the funding model fairer for all taxpayers.

➤ **Conclusion**

The assessment demonstrates that the level of equity among individual taxpayers in the regional service would be improved through the use of a property value tax, in place of the current property parcel tax, to determine and collect service payments from properties. Equity among individual taxpayers would also be improved through the introduction of a DCC to assist in funding land acquisitions and development.

Effectiveness

Does the current funding model, with its reliance on property taxes as the sole source of revenue, allow the RDN to raise sufficient funds for the service, given the expectations of residents and elected officials, and in view of key challenges? It is difficult to answer this question definitively until the RDN has completed its anticipated update (beginning in 2018) to the *Regional Parks & Trails Plan*. The process through which the *Plan* is updated will:

- clarify or confirm the fundamental purpose and goals of the service, as determined by the Board
- review the existing inventory of parks and trails
- confirm and articulate the anticipated need for additional regional parks and trails, based on the expectations of the broader regional community for the

protection of natural areas, and for opportunities to connect with, be active in, and learn about the natural environment

- identify the most important types of properties to acquire on a go-forward basis
- refine existing acquisition criteria
- consider parks and trails development needs
- examine staffing and other operational and maintenance resource levels
- quantify the anticipated costs of acquisition, development and operations in the coming years

Through the update to the *Plan*, the Board will be able to determine whether the current funding model can be used to raise sufficient revenues, or whether additional revenue-generating tools should be considered.

The need for an updated *Plan* notwithstanding, it does appear to be the case, based on consultations and the review of materials, that the service requires more funds to meet existing needs and expectations related, in particular, to acquisition and development. In plain terms, people in the RDN want more regional parks and trails, and they want to be able to use them. More funds could be obtained by simply increasing the taxes charged against property owners. Increases of this sort may, indeed, be part of the solution;¹² however, funds could also be raised by introducing a regional parks and trails DCC (referred to earlier), and by undertaking efforts to increase, where possible, fees for special events, film permits and other services.

It is not being suggested that the RDN introduce a wide range of fees for those who use the regional parks and trails system. Too many user fees may inadvertently undermine the ability of all residents in the RDN to access the system. What is being suggested is to increase user fees for specific permits in an effort to increase the total amount of user fee revenue available in the service. At present, the RDN generates essentially no such revenues. By contrast, user fees at other regional districts, including the CRD and MVRD, account for 5% to 8% of total service revenues.

➤ Conclusion

The current funding model does not appear to provide sufficient funding to meet the expressed expectations and interests for the service. The RDN should consider introducing a regional parks and trails DCC to increase and diversify funding. User fees for special events, filming and other permits should also be reviewed and increased where warranted. The Regional District may also need to increase the amount it collects in service tax revenues from the service area in order to meet increasing level of service demands.

¹² The tax amount collected per property in the RDN is much lower than the amount collected on an average property in the CRD and RDCO.

Transparency

Support for regional services increases when funding models are transparent in their use of tax dollars — that is, when funds raised are used in accordance with their stated purpose. At the RDN, transparency in the Regional Parks & Trails Service funding model is enhanced by the separation of acquisition and development funds from operational funds. This separation should continue, irrespective of the approaches taken to cost allocation and taxation.

Questions related to transparency often arise in regional park services in discussions on spending for land acquisition and park development. Many of the regional districts reviewed for the assignment — CRD, RDCO, MVRD and CVRD stand out — are facing pressures to develop lands that have already been acquired. All of these regional districts are looking to their acquisition reserve funds as much-needed sources of revenue. In certain cases — RDCO, for example — the purpose of the reserve fund clearly includes parks and trails development costs. In other regional districts the flexibility is less clear. Officials in these other places who wish to use reserve funds for both acquisition and capital are finding it necessary to seek explicit approval from electors, who may consider the funds to be earmarked for acquisition only.¹³

In the RDN, transparency in the use of capital funds is not a major concern. The reserve fund that is used to assist in both the cost of acquisition and the cost of development is clearly identified in key RDN materials as the Regional Parks Acquisition and Capital Development Fund. In certain materials (e.g., service budget sheets), the fund is identified in short-hand as an acquisition fund. These instances should be corrected to include reference to major capital. In all instances, the Fund should be referred to as the "Regional Parks Acquisition and Capital Development (Reserve) Fund".

➤ Conclusion

The RDN should continue to separate acquisition and development funding from operating funding, irrespective of the approaches taken to cost allocation and taxation. The RDN should also ensure that monies held in its Regional Park Acquisition and Capital Development (Reserve) Fund are identified consistently as funds for both acquisition and development.

RECOMMENDATIONS ON REGIONAL SERVICE FUNDING MODEL

Based on the assessment of the RDN's current funding model for the Regional Parks & Trails Service, the following recommendations are provided for the Board's consideration:

- THAT the Board work with participating jurisdictions to amend *Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)* to allocate land acquisition and development costs among

¹³ It is understood that the CRD will be appealing to electors on this point in 2018.

service participants on the combination (50-50) of converted assessment and population, rather than number of parcels.

- THAT the Board work with participating jurisdictions to amend *Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)* to replace the property parcel tax for acquisition and development costs with a property value tax.
- THAT the Board work with participating jurisdictions to amend *Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)* to allocate service operating costs among service participants on the combination (50-50) of converted assessment and population, rather than population alone.
- THAT the Board direct staff to undertake a survey of regional parks and trails users, at key times of year, every three-to-five years, to identify and track the home jurisdictions of users.
- THAT the Board, pursuant to section 559(2) of the *Local Government Act*, introduce a Development Cost Charge to assist in raising funds required for parkland acquisition, and parkland improvements.
- THAT the Board direct staff to review the existing permit fees charged for special events, filming, and commercial activities, and to propose a new revenue-generating fee schedule.
- THAT the Board continue its approach of collecting land acquisition and capital development funds separately from funds that are collected to support planning, operations and maintenance.
- THAT the Board clarify in all materials that monies held in the Regional Parks Acquisition and Capital Development (Reserve) Fund are intended both for land acquisition and capital project purposes.

CHAPTER I.4

ADDITIONAL ISSUES

Over the course of the *Service Review*, certain additional issues arose that should be considered, but that do not fit neatly into the discussions on funding models. Two issues emerged as being particularly important to examine:

- the potential for the RDN to assume responsibility, in whole or part, for municipal parks that have regional park characteristics
- the potential for an integrated approach to parks and trails planning that would take into account municipal and electoral area park systems, along with the regional parks and trails system

Each of these issues is reviewed briefly in this chapter of the report.

MUNICIPAL PARKS

Municipalities are responsible for providing a range of local parks to their respective populations. Some of the parks are acquired and designed to provide benefit to small areas within cities, typically one or two neighbourhoods. These parks are often referred to as "tot lots" or neighbourhood parks. Municipalities also provide larger parks that are designed to benefit section of cities, and that may host sports equipment, playgrounds and other improvements. These parks are in some cases referred to as district parks. Several municipalities provide more significant parklands and trails with large catchment areas that may transcend municipal boundaries. These properties, often called city parks or destination city parks, may feature high quality sport fields, field houses and other facilities.

In addition to these various municipal park types, a number of municipalities provide large parks and trails that appear to many observers to be regional in nature. These parks may protect significant natural areas, ecosystems and habitats, and may showcase important regional landscapes. They often feature trail systems through the lands, but are otherwise essentially undeveloped. Some are large enough to protect and promote the provision of ecosystem services.

There are many examples of municipalities in the province that provide these region-like parks. The Cities of Surrey, Burnaby, Delta and Richmond, and the District of North Vancouver in the MVRD all have significant, natural parks that complement the regional park system. Kelowna, Kamloops, Vernon and Salmon Arm are a few of the many examples from the Interior. On the Island, Victoria and Saanich are good examples, as is the City of Nanaimo in the RDN with parks such as Westwood Lake and Linley Valley, and conservation areas such as Buttertubs Marsh.

Several regional districts have been faced with the prospect of assuming responsibility for municipal parks that possess regional park qualities. In general, regional districts have been reluctant to embrace such parks for a number of reasons:

- A decision to accept responsibility for one municipal park inevitably leads to requests from other municipalities, as well as raised expectations. Many municipalities, as noted earlier, control and operate parks that have regional qualities, including large benefitting areas. A regional district that agrees to take responsibility in one case could quickly find itself overwhelmed by demands to take responsibility over others' parks.
- The original decisions to acquire the land, establish and operate a park, and make ongoing investment in the park, were made by the municipal council, not the regional district board. Had the regional board been involved in past decisions, a different type of park may have emerged, established to address a different purpose and achieve different goals.
- There is not always agreement with respect to what constitutes "regional qualities". Improvements (e.g., paved trails) or activities in some large municipal parks may be not support the purpose of the regional parks and trail system.
- Municipalities that do transfer responsibility over key parks to the regional district may have a difficult time "letting go". Decisions made by the regional board may not be supported by the municipality or its residents that use the park. In such cases, the potential for conflict between jurisdictions would be high.
- Municipalities that transfer control through leases or licenses of occupation may decide that they want control back at the end of the contract. In these cases, the regional district and park users could face uncertainty and disruption over the future purpose of the park and the goals the park was intended to achieve. In Metro Vancouver, the City of Burnaby leased Burnaby Lake to the MVRD to operate within the regional park system. Burnaby has decided to not renew the lease in 2021. This decision has caused anxiety among park user groups and others who value the park's ecosystem services and other features, and who view the regional park system as an important source of protection.

The MVRD is proceeding cautiously with respect to Burnaby Lake Park, and on the broader issue of assuming responsibility for other municipal properties. No other regional district surveyed for the study is contemplating or encouraging any transfer of existing municipal parks.

In the consultation with decision-makers at the RDN, the transfer of responsibility issue did not generate discussion or interest. The creation of a park in the Lantzville Foothills was identified as a topic for further discussion between the municipality and the RDN. No such park, however, exists today.

INTEGRATED PLANNING

There is considerable interest on the part of RDN and its member municipalities to integrate regional and local parks and trails planning on a go-forward basis. Integration could help to link parks and trails systems, reduce overall planning costs, and achieve sub-regional and region-wide environmental and active-living goals. Integrated planning also would help jurisdictions to identify important parks and trails gaps, and set acquisition and development priorities accordingly.

The process for updating the *Regional Parks & Trails Plan* in 2018 provides an opportunity to bring together planning efforts.

RECOMMENDATIONS

Based on the discussion on the additional issues raised in this chapter, the following recommendation are presented to the Board for consideration:

- THAT the Board refrain from assuming responsibility, in whole or part, for municipal parks that may possess regional park characteristics.
- THAT the Board direct staff to work with their counterparts in the Regional District's member municipalities on developing and implementing an integrated planning framework for regional and local parks and trails.

CHAPTER I.5

ADDITIONAL ISSUES

Part I of this report has presented an assessment of the funding model in place for the RDN's Regional Parks and Trails Service. The recommendations presented for the Board's consideration are summarized in Figure I.5.1.

Figure I.5.1
Summary of Recommendations

Topic	Recommendations
Regional Service Funding Model	<p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to allocate land acquisition and development costs among service participants on the combination (50-50) of converted assessment and population, rather than number of parcels.</p> <p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to replace the property parcel tax for acquisition and development costs with a property value tax.</p> <p>THAT the Board work with participating jurisdictions to amend <i>Regional District of Nanaimo Regional Parks and Trails Service Area Conversion Bylaw No. 1231 (2001)</i> to allocate service operating costs among service participants on the combination (50-50) of converted assessment and population, rather than population alone.</p> <p>THAT the Board direct staff to undertake a survey of regional parks and trails users, at key times of year, every three-to-five years, to identify and track the home jurisdictions of users.</p> <p>THAT the Board, pursuant to section 559(2) of the <i>Local Government Act</i>, introduce a Development Cost Charge to assist in raising funds required for parkland acquisition, and parkland improvements.</p> <p>THAT the Board direct staff to review the existing permit fees charged for special events, filming, and commercial activities, and to propose a new revenue-generating fee schedule.</p> <p>THAT the Board continue its approach of collecting land acquisition and capital development funds separately from funds that are collected to support planning, operations and maintenance.</p>

Topic	Recommendations
	<p>THAT the Board clarify in all materials that monies held in the Regional Parks Acquisition and Capital Development (Reserve) Fund are intended both for land acquisition and capital project purposes.</p>
Additional Issues	<p>THAT the Board refrain from assuming responsibility, in whole or part, for municipal parks that may possess regional park characteristics.</p> <p>THAT the Board direct staff to work with their counterparts in the Regional District's member municipalities on developing and implementing an integrated planning framework for regional and local parks and trails.</p>

PART II

REGIONAL DISTRICT OF NANAIMO COMMUNITY PARKS AND TRAILS SERVICES

**RDN
PARKS FUNDING
SERVICES REVIEW
REPORT**

NEILSON-WELCH
CONSULTANTS TO GOVERNMENT

CHAPTER II.1 CURRENT SERVICE

There are eight separate Community Parks & Trails Services, one in each of Electoral Areas A, B, E, F, G and H, and two in Electoral Area C. The services exist to:

- provide opportunities and amenities for outdoor leisure and recreation
- protect local natural features
- provide trail connections to parks, public places, beaches and other community destinations
- protect important local heritage and cultural features

Together, the services offer 202 parks that cover over 600 ha of land. With the exception of the 286 ha 707 Community Park on Gabriola Island (Electoral Area B), the individual community parks are relatively small in size, and are acquired, designed and developed to benefit local communities within the electoral area. There are very few trails at present in any of the services.

With advice and guidance from local advisory committees, the RDN undertakes a full range of functions under each Community Parks & Trails Service, including park planning, land acquisition, parks and trails development, and ongoing operation and maintenance of parks and trails. Parkland acquisition efforts are guided by park- and trail-related policies in each electoral area's *Official Community Plan*, and by other considerations. In the District 69 electoral areas, acquisition criteria and scorecards are outlined in the 2014 *Community Parks & Trails Strategic Plan (Electoral Areas E, F, G & H)*.

A few community parks in the different services have management plans — 707 Community Park is an example. By and large, however, management plans are not in place and are not required for most parks and trails.

FUNDING MODEL

The transfer of land for community park purposes through the development process is the primary method used by the RDN to acquire parks and trails for the eight Community Parks & Trails Services. Section 510 of the *Local Government Act* requires every owner of land that is being subdivided to provide, without compensation, 5% of the land for parks.¹⁴ The same section allows the RDN to require owners to provide monies in lieu of dedication. The monies are placed in reserve funds where they are used in accordance with policies in the specific electoral area's *Official Community Plan* related to community parks and trails. Where possible, dedicated lands or funds-in-lieu are used by the RDN to leverage additional resources through partnerships with other agencies. The Regional District

¹⁴ Section 510(3) provides some exemptions related to number of lots created, size of lots being created, and subdivision that results in the consolidation of lots.

is currently pursuing several partnership opportunities, for example, with School Districts 68 and 69.

Community parks and trails are developed, for the most part, using a combination of senior government grants and property tax revenue. Grant revenues consist primarily of Community Works Fund (CWF) monies, provided to the RDN for its electoral areas through the Federal Gas Tax Sharing program. CWF funds may be used within electoral areas for a wide variety of infrastructure works, including parks and trails improvements.

Figure II.1 shows the CWF funds spent under the Community Parks & Trails Services in the past two years. Certain electoral areas, it should be noted, spent additional CWF funds to assist with portions of regional trails that traverse the specific electoral areas. Electoral Area G, for example, contributed \$110,000 in CWF monies to the E&N Rail Regional Trail to assist with the portion of the trail within Area G. Area F's contribution to the same trail (referenced earlier) totaled \$350,000. Electoral Area A contributed \$18,000 to the Morden Colliery Regional Trail (and \$42,000 in earlier years). None of these costs is reflected in Figure II.1.1.

Figure II.1.1
Community Works Fund Support for
Community Parks & Trails (2016 & 2017)

Area & Local Project	2016	2017
Area A SFN Sport Court Upgrade		300,000
Area B Gabriola Village Trail Huxley Park Upgrades Skatepark Whalebone Park Beach Access	17,745	7,678 234,000 12,000 25,000
Area C	n/a	n/a
Area E Claudet Community Park Blueback Community Park Es-hw Sme~nts Park Jack Bagley Field	19,100 50,000 22,140	7,860 10,000
Area F Cranswick Road Trail Carruthers Road Trail	13,110	18,010
Area G Area H	n/a n/a	n/a n/a

Operations and maintenance for the each Community Parks & Trails Service are funded by property value tax revenues that are generated within the specific service area (which, in every electoral area except for Area C, consists of the entire electoral area). Value taxes are levied against all properties (land and improvements). Figure II.1.2 shows the 2017 value tax rate and total requisition for each service area.

Challenges Related to Funding Model

One of the key funding model challenges facing the Community Parks & Trails Services concerns the cost of parks and trails development. Most of the community parks in the electoral areas are undeveloped in their natural state. As populations and the levels of residential development increase, expectations for outdoor recreation amenities and other improvements are likely to increase, as well. There will be pressure on the RDN to make funds available for increased park development. Added to the challenge is the concern noted earlier in the discussion on regional parks and trails related the rising cost of materials and labour.

A second challenge relates to the allocation of RDN staffing resources among the services in the different electoral areas. Areas may seek assurance that they are getting their "fair share" of resources, and/or not paying for services used by others.

Increasing land values may be less of an issue for the Community Parks & Trails Services than for Regional Parks & Trails, because of the reliance of parkland dedication in the acquisition of local parkland. Land owners in the electoral areas who wish to subdivide for development must dedicate 5% of the land, or provide (at the option of the RDN) a payment-in-lieu of dedication equal to the value of the land. The 5% requirement applies irrespective of the value of the land. The value of payments-in-lieu of dedication increases in tandem with the value of land.

Figure II.1.2
Community Parks & Trails Services
Value Tax Rate and Requisition (2017)

Jurisdiction	Value Tax Rate	Converted Assessment	Total
Electoral Area A	0.150	132,107,639	198,490
Electoral Area B	0.224	123,656,725	277,000
Electoral Area C (Extension)	0.095	72,499,538	68,807
Electoral Area C (East Wellington)	0.236	37,989,937	89,679
Electoral Area E	0.067	212,954,769	142,080
Electoral Area F	0.109	142,929,996	156,240
Electoral Area G	0.068	186,536,303	126,623
Electoral Area H	0.165	112,747,856	186,520
Total All Electoral Areas			1,245,439

CHAPTER II.2

FINANCIAL TOOLS

This chapter reviews the range of tools available to regional districts in British Columbia to fund the various activities that are undertaken in community parks and trails services. Most of the tools are in use already at the RDN. The information presented draws on the comparative research that was conducted for the *Service Review*.

FINANCIAL TOOLS

Financial tools are identified under each of the main service components, namely land acquisition, parks and trails development, and planning, operations and maintenance.

Land Acquisition

Tools available specifically for local parks and trails acquisition are outlined in Figure II.2.1.

Figure II.2.1
Land Acquisition

Tools	Description
Dedication through Subdivision	All regional districts with community parks and trails services acquire lands for local parks and trails through the subdivision process, pursuant to section 510 of the <i>Local Government Act</i> . Included under this tool is the option, available in electoral areas with OCP policies on park location and type, to take monies-in-lieu of lands from owners seeking subdivision approval.
Dedication through Rezoning	The rezoning process offers another opportunity to regional districts for the acquisition of lands for community parks and trails. 707 Community Park on Gabriola Island was created using lands that were dedicated through rezoning in exchange for density transfers.
Land Transfer from Governments	Regional districts acquire some community parks and trails through transfers from senior governments. The RDCO recently acquired important lands from the province through long-term lease in the Westside Electoral Area. Other regional districts have acquired beach access points, in part, through the transfer of road ends from the Ministry of Transportation and Infrastructure (MOTI). MOTI provides rights-of-way to regional districts through permits or licenses of occupation. The CRD has a memorandum of understanding in place with MOTI that focuses on licenses of occupation, and that outlines the rights and responsibilities of both parties. The CSRD also obtains access from MOTI in the form of licenses of occupation. Rights-of-way are provided by senior governments in some cases to

Tools	Description
	provide community trails, including trails that make use of local roads in electoral areas (such roads are owned and controlled by MOTI).
Contributions from Others	Non-profit community associations, private companies and individuals provide lands in certain instances for local parks and trails. In some cases, ownership of the lands is transferred through title; in other cases, transfers of responsibility for operations occur using leases and licenses of occupation.
Development Cost Charges	Regional districts have the authority to impose DCCs to assist in the cost of acquiring (and developing) community parks and trails. Of the regional districts surveyed for this report, only the Comox Valley RD has a local parks DCC program in place. RDCO had a program for the former Westside Electoral Area prior to 2006. This program, however, transferred to the West Kelowna municipality upon incorporation.
Property Value Taxes	Property value taxes are used primarily for planning, operations and management, but are also relied on in some cases to assist with land acquisition. Property tax revenues were identified by the CVRD as an important acquisition resource.

Parks and Trails Development

Figure II.2.2 identifies the tools available to assist in developing community parks and trails.

Figure II.2.2
Parks and Trails Development

Tools	Description
Senior Government Grants	Senior government grants are relied on as a significant source of funding for community parks and trails development in many regional districts. The most important fund is the Community Works Fund (CWF), paid to municipalities through the Federal Gas Tax Sharing program. Other infrastructure funds also provide development funds. Several local parks in the electoral areas of many regional districts received funding under the aforementioned Canada 150 fund.
Contributions through Partnerships	Regional districts may receive assistance with development costs from school districts and other agencies under agreements to co-develop and provide local parks.
Amenities through Rezoning	Regional districts can negotiate amenity contributions from land owners during the rezoning process to assist with capital projects in local parks.
Contributions from Others	Regional districts may receive funds for capital works (e.g., playgrounds, tennis courts, etc.) from local non-profit associations. Several

Tools	Description
	associations conduct fundraising campaigns to assist with specific development projects. Private companies will, at times, be another source of such funds. Donations from individuals are a third type of contribution for parks and trails development. Donations may be made as part of fundraising campaigns, as stand-alone gifts, or through commemorative and other programs aimed at providing furniture (e.g., benches) and equipment (e.g., playgrounds).
Development Cost Charges	DCCs may be imposed to assist in funding local parks and trails development, in addition to acquiring land. As noted previously, however, only one of the regional districts examined for this report (Comox Valley RD) has community parks and trails DCCs in place.
Property Value Taxes	Property value taxes are used in most regional districts to assist with local parks and trails development.

Planning, Operations and Maintenance

Regional districts rely primarily on property value tax revenues to pay for the planning, operation and maintenance of community parks and trails. Cost-sharing agreements with school districts, contributions from community associations, and park user fees represent other tools. Where available, however, these other sources typically offset the need for taxes only to a modest degree.

CHAPTER II.3

ASSESSMENT OF FUNDING MODEL

This chapter provides an assessment of the RDN's funding model that is in place today to pay for the eight Community Parks & Trails Services. The assessment is conducted using the same evaluation criteria that were used in the assessment of the regional service. Recommendations for the Board to consider are put forward. The experiences of other regional districts, identified through the comparative research, inform both the assessment and the recommendations.

COMMUNITY PARKS & TRAILS FUNDING MODEL

The RDN's Community Parks & Trails Services was profiled earlier in the report in Chapter II.1. The services' funding model can be summarized by the following points:

- Lands for community parks and trails are acquired, primarily, using the authority in section 510 of the *Local Government Act* dealing with parkland dedication, or payments-in-lieu, at subdivision.
- Community parks and trails are developed using a combination of CWF monies, other senior government grant programs, and property tax revenues. Contributions from other agencies also assist.
- Funds for planning, operations and maintenance are raised using property value taxes, unique to each service area.

ASSESSMENT OF COMMUNITY SERVICES FUNDING MODEL

The assessment of the local services funding model makes use of the same evaluation criteria presented earlier for the regional service model.

Equity (Jurisdictions)

Each of the eight Community Parks & Trails Services in the RDN has its own service area and budget. Most of the costs incurred to provide each service are determined by taxpayers in the specific, local service area, through the service's local advisory commission and the Electoral Area Director. Costs determined in this way are unique to the specific service, and are not allocated across other areas.

The cost of Parks and Recreation staff assigned to support the Community Parks & Trails Services is the exception. This cost is allocated across the electoral areas in equal portions (\$80,234 in 2017).¹⁵ This method of allocation may, at first glance, seem unfair given differences between and among the local services. The approach, however, can be supported by a number of points:

¹⁵ The two services in Electoral Area C are each billed one-half of one portion. The result is that base staff costs are allocated equally among the seven electoral areas.

- All of the local services require a certain base amount of parks staff time to administer and operate properly. Every service requires and receives this base support and is expected to pay for it.
- Staff are required periodically to spend considerable amounts of time and energy on specific tasks in each of the electoral areas. Examples of such tasks include the processing of subdivision dedications, the assessment and development of partnership opportunities, and the management of CWF-supported capital projects. The workload associated with any particular service shifts over time in response to needs that arise. All of the services, however, make significant demands on staffing resources from time to time.
- Much of the cost incurred by the RDN to operate and maintain community parks and trails, and to undertake capital projects, relates to work that is performed by contractors under park-specific contracts. These costs are kept separate from the RDN staffing costs, and are not shared among local service areas.
- A consistent, equal allocation of costs, rather than a changing and erratic approach that attempts to reflect varying workload projections in each service every year, promotes funding and taxation stability.

Figure II.1.1 in Chapter II.1 of the report provides information on the spending of CWF monies in the different electoral areas in 2016 and 2017. The significant differences in the chart may suggest to some that there is a level of inequity across the local services. No such inequity, however, exists. The CWF is a long-term, annual program that allocates federal gas tax revenues to all electoral areas on a *per capita* basis. CWF monies are not unconditional grants since they must be used for capital projects that fit into one of the eligibility categories. The range of categories is sufficiently broad, however, to provide electoral areas with considerable autonomy over spending.

The significant differences in CWF spending between and among electoral areas in Figure II.1.1 indicate only that some electoral areas have chosen to spend their CWF grants on projects in services other than community parks and trails. The differences do not point to any major inequity.

➤ Conclusion

Based on the assessment of the local services against the inter-jurisdictional equity criterion, the RDN should refrain from making changes to its current approach to allocating the cost of staff assigned to support the community services.

Equity (Individual Taxpayers)

The property tax that is imposed to help pay the cost of each Community Parks & Trails Service is a value tax, levied to all property owners within the service area on the basis of assessment (land and improvements). This arrangement provides for equity among individual taxpayers.

Development that occurs in the electoral areas contributes to the provision of parkland and trails through the subdivision dedication provision of the *Local Government Act*. The Regional District has the authority to require new development to pay a DCC, in addition to dedicating land during subdivision, to assist further in meeting land acquisition costs, and in helping to fund parks and trails development. For a DCC to be viable in helping to fund acquisition costs, however, the RDN would need also be providing funds for acquisition (DCCs are intended to pay only a portion of the total cost). At present, the RDN relies almost entirely on the subdivision dedication process and transfers from other agencies to acquire parks and trails at the local level.

A DCC is an option to consider for help in funding local parks and trails improvements. In several electoral areas in the RDN, development is strong and would almost certainly be able to pay a modest DCC for park and improvements. The introduction of a charge in all or some of the electoral areas would bring much-needed revenue to meet increasing demands for parks and trails infrastructure. The charge would also promote equity among taxpayers.

➤ **Conclusion**

Based on the assessment of equity between and among individual taxpayers, the RDN should leave unchanged its reliance on property value taxes for the local services. The RDN should consider introducing a DCC specifically for community parks and trails improvements.

Effectiveness

It is difficult to determine whether the current funding model allows the Regional District to raise sufficient funds to meet all service needs. It can be noted, however, that rising expectations and increased growth will result in greater needs, including the potential need for greater amounts of service funding. Tax rates can be raised, as always; however, additional funding sources in the form of DCCs (as noted) and user fees may also be available. The RDN has a strong track record of collaboration with other agencies to help contain costs. These efforts will continue to benefit the services.

➤ **Conclusion**

A new DCC should be considered. Initially, the DCC should be focused on improvements. Over time, the tool could be expanded to include land acquisition in the event that the Regional District begins to fund community park acquisition efforts directly with tax dollars.

Transparency

The funding model for the *Community Parks & Trails Services* is relatively simple and straightforward. Funds raised in each service area are spent only on community parks and trail expenses incurred in that area. Transparency does not appear to be an issue.

RECOMMENDATIONS ON COMMUNITY SERVICES FUNDING MODEL

Based on the assessment of the RDN's current funding model for the Community Parks & Trails Services, the following recommendations are provided for the Board's consideration:

- THAT the Board retain its current practice of allocating staffing costs equally across the Electoral Areas.
- THAT the Board continue to raise service funds using property value taxes.
- THAT the Board, pursuant to section 559(2) of the *Local Government Act*, introduce local Development Cost Charges to assist in raising funds required for parkland improvements.

CHAPTER II.4

SUMMARY OF RECOMMENDATIONS

Part II of this report has presented an assessment of the funding model in place for the RDN's eight Community Parks and Trails Services. The recommendations presented for the Board's consideration are summarized in Figure II.4.1.

Figure II.4.1
Summary of Recommendations

Topic	Recommendations
Community Services Funding Model	<p>THAT the Board retain its current practice of allocating staffing costs equally across the Electoral Areas.</p> <p>THAT the Board continue to raise service funds using property value taxes.</p> <p>THAT the Board, pursuant to section 559(2) of the <i>Local Government Act</i>, introduce local Development Cost Charges to assist in raising funds required for parkland improvements.</p>